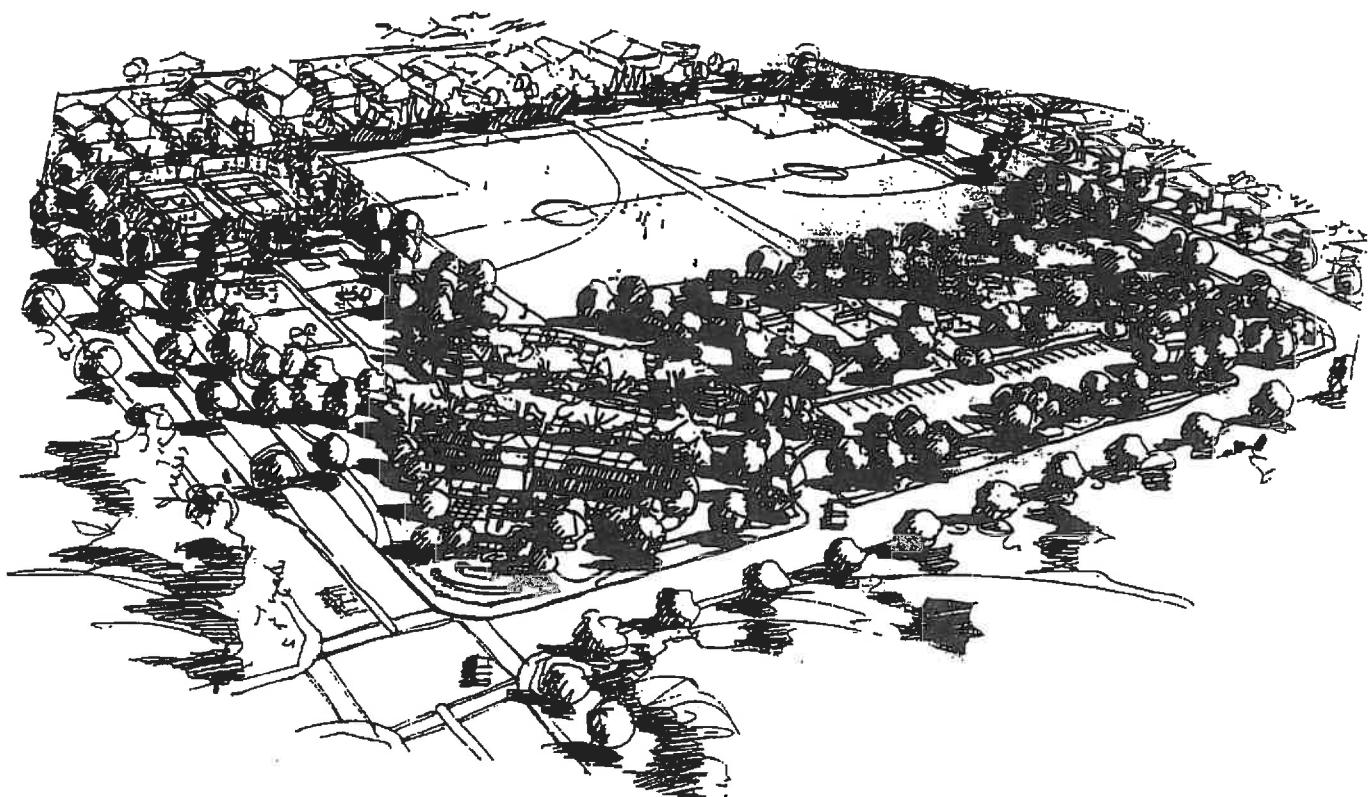


California Landings Specific Plan
Amendments Added

Specific Plan Regulations	Ordinance 1124	August 16 th 1994
Amendment #1	Ordinance 1207	December 17th 1996
Amendment #2	Ordinance 1372	March 19 th 2002
Amendment #3	Ordinance 1809	September 24 2019

California LANDINGS

AMENITY ALTERNATE



FINAL SPECIFIC PLAN

August 17, 1994

Draft Submitted: November 3, 1993

Final Submitted: June 14, 1994

Final Certified: August 17, 1994

CALIFORNIA LANDINGS FINAL SPECIFIC PLAN

August 17, 1994

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1.0 EXECUTIVE SUMMARY

1.1 LOCATION AND SETTING

The California Landings Specific Plan is located in the northern portion of the City of Fontana as depicted in the Regional Map (see Exhibit 1). The 223 acre site is bounded by Highland Avenue (proposed Route 30) to the north and is bounded by existing residential and vacant parcels to the east. Walnut Avenue and the Rancho Fontana Planned Community (currently vacant adjacent to the proposed project) are located to the south and Hemlock Avenue to the west (see Exhibit 2, Vicinity Map and Exhibit 5, Specific Plans in the Area).

The proposed site is composed of five separate parcels. The first parcel consists of the abandoned Gilfillan Airfield (approximately 144 acres) which contains six abandoned single-story structures. The second parcel is a 64 acre former chicken ranch, now vacant property. In addition, there are three out-parcels with two existing residences and one metal building comprise 15 acres of the Specific Plan area. (See the Exhibit in Appendix, Section 7.1, for a graphic depiction of these parcels). These three parcels are owned by separate entities other than the project applicant, Kaufman and Broad, but are included in the Specific Plan at the request of the City of Fontana in order to facilitate area-wide planning. The site is characterized as relatively flat with slopes at approximately 2% in a southwesterly direction. It is generally considered to be developed due to the previous airport and chicken ranch facility.



Exhibit 1

REGIONAL MAP



1-2

California
LANDINGS

Kastman & Broad

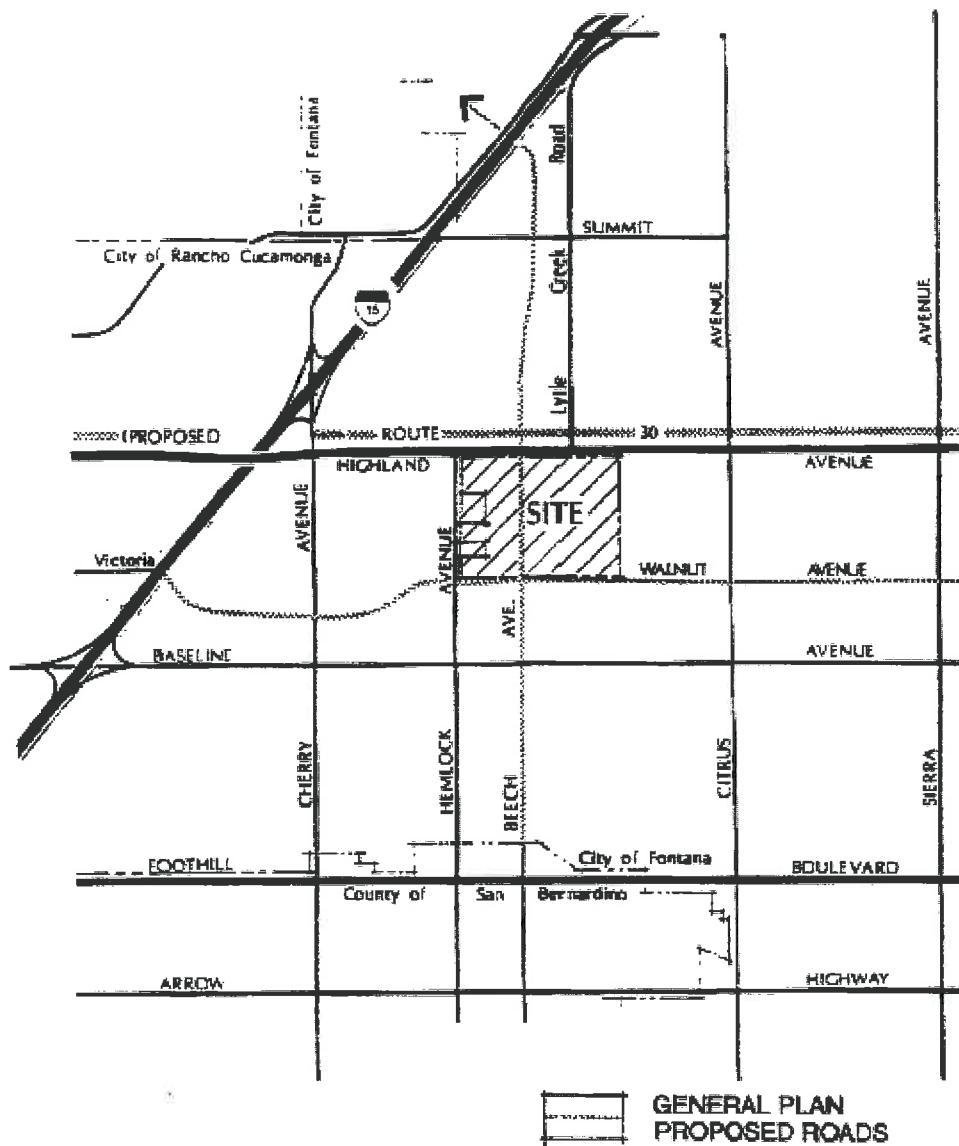


Exhibit 2

VICINITY MAP



EKA

California
LANDINGS

Kaufman & Broad

1.2 PROJECT SUMMARY

This document contains all of the necessary information to implement the California Landings Specific Plan.

The California Landings project, a 223-acre Specific Plan, can be summarized by the following points:

- Kaufman & Broad is the major landowner controlling 208 acres (93.2%) with 15 acres (6.8%) belonging to other ownerships (See Appendix 7.1 for detail). The Kaufman & Broad parcel allows a maximum of 750 dwelling units on 155.7 residential acres for a density of 4.8 dwelling units per acre (750 du/155.7 residential acres).
- The property under Other Ownership allows a maximum of 41 dwelling units on 15.0 acres for a density of 3.0 units per acre.
- It provides 14.8 acres for single family cluster residential development at a maximum of 194 dwelling units for a density of 13.1 units per acre.
- It provides 17.7 acres of community roads.
- It includes 10.0 acres for park and bike trails (See Section 3.5, page 3-18).
- It provides park design concepts that feature an aviation theme within the large central park. The theme recalls the historic use of the majority of the California Landings site as an airport.
- It is designed to implement the City of Fontana General Plan designations for the property. The general plan for this project indicates the **Planned Residential Community (R-PC)** designation.
- It is designed to achieve effective separation of uses and independent neighborhood access, both of which are achieved largely by the arterial system.
- It incorporates a series of clearly presented land use categories, site

development standards and public facility/service plans.

It contains design guidelines for assuring quality development incorporating quality design concepts and consistent community design theme.

It is phased to facilitate financing, marketing and to assure timely provision of infrastructure.

It will provide an elementary school site for future residents of the community and residents *in* the adjacent neighborhoods.

LAND USE PLAN

AMENITY ALTERNATIVE

STATISTICAL SUMMARY

LAND USE	STATE	ROUTE	30	NET AC	DU
KAUFMAN & BROAD					
RESIDENTIAL					
SINGLE FAMILY DETACHED (SFD)					
NEIGHBORHOOD:					
A 7,200 S.F.					56
B 6,200 S.F.					33.5
C 6,200 S.F.					33
D 6,200 S.F.					11.7
E 6,200 S.F.					18.6
F 5,000 S.F.					91
					40.2
					29.7
					105
RESIDENTIAL SUBTOTAL					155.7
DETACHED & ATTACHED					14.8
CLUSTER RESIDENTIAL					19.4
ELEMENTARY SCHOOL					10.0
PARK					10.0
CIRCULATION					17.7
PROJECT TOTAL					208.2
444					
OTHER OWNERSHIPS					
RESIDENTIAL					
SINGLE FAMILY DETACHED (SFD)					
NEIGHBORHOOD:					
1 10,000 S.F.					87
2 10,000 S.F.					46
RESIDENTIAL SUBTOTAL					13.5
CIRCULATION					1.5
OTHER OWNERSHIP TOTAL					15.0
SPECIFIC PLAN TOTAL					223.2
95					
LEGEND					
SPECIFIC PLAN BOUNDARY					
KAUFMAN & BROAD OWNERSHIP					

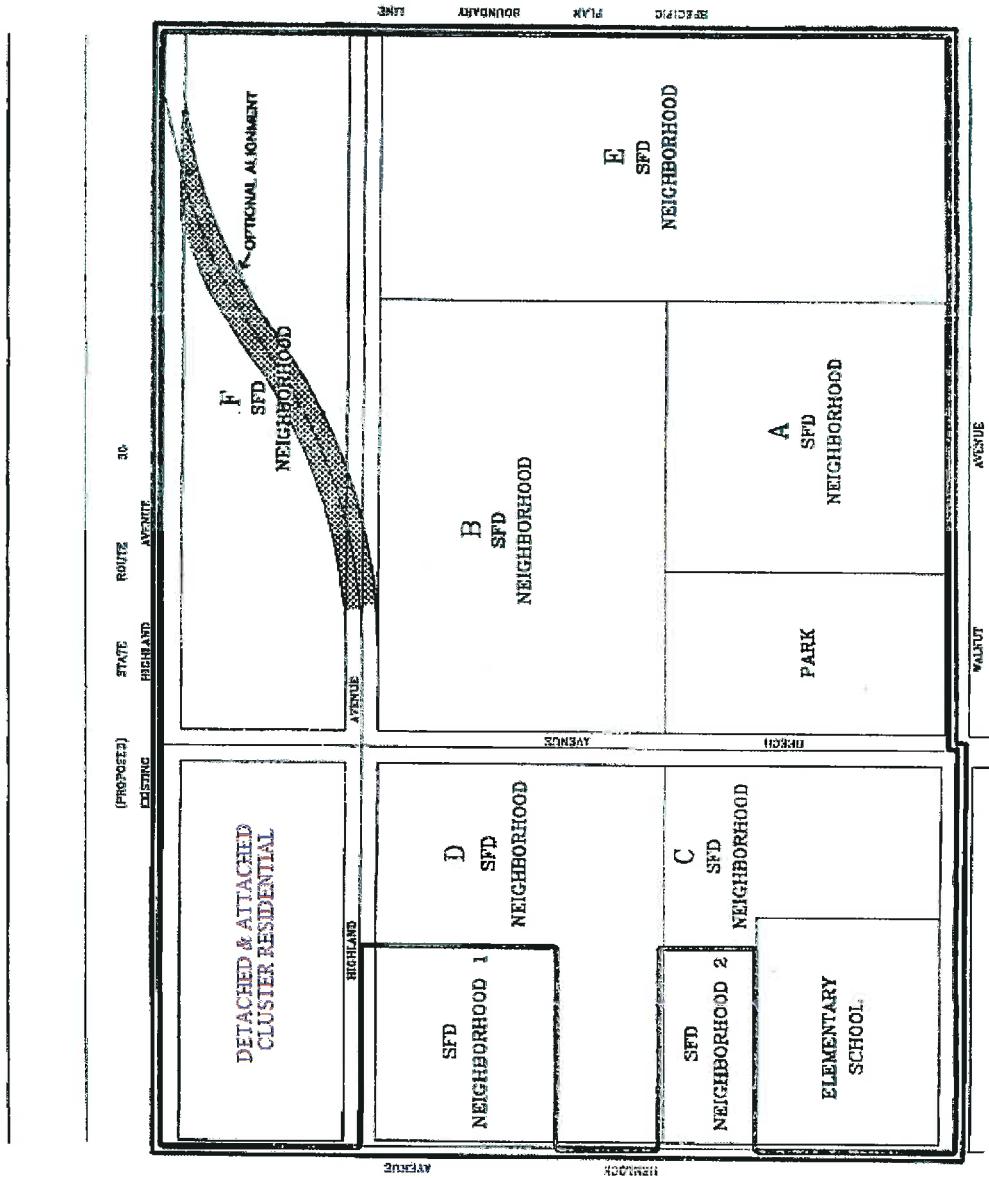


Exhibit 3

California
LANDINGS

Kaufman & Broad

TABLE 1.1
LAND USE PLAN SUMMARY

LAND USE	ACRES	DU	DENSITY (DU/ACRE)	% OF SITE
<u>KAUFMAN & BROAD</u>				
RESIDENTIAL				
Single Family Detached (A-F)	155.7	750	4.8	69.7%
DETACHED & ATTACHED CLUSTER RESIDENTIAL	14.8	--	--	6.6%
ELEMENTARY	10.0	--	--	4.5%
PARK	10.0	--	--	4.5%
CIRCULATION	<u>17.7</u>	--	--	<u>7.9%</u>
SUBTOTAL	208.2	750	--	93.2%
<u>OTHER OWNERSHIP</u>				
RESIDENTIAL				
Single Family Detached (1 & 2)	13.5	41	3.0	6.1%
CIRCULATION	<u>1.5</u>	--	--	<u>0.7%</u>
SUBTOTAL	15.0	41	--	6.8%
TOTAL	223.2	791	--	100.0%

1.3 A USER'S GUIDE TO THE SPECIFIC PLAN

The plan contains a large amount of information and guidance. It is designed to facilitate use by those who wish to build projects in conformance with the plan, staff personnel who must review project proposals and decision makers who are called upon to act on these projects.

A. Contents and Sequence of Sections of the Specific Plan

Section 1. Executive Summary of Sections of the Specific Plan: provides an executive summary and a broad overview of the plan and describes its use.

Section 2. Planning Framework: provides background and a purpose statement, description of existing and proposed land uses, as well as a project description. In addition, general plan compliance is demonstrated and a summary of amenities and standards that exceed City Code is presented.

Section 3. Community Development: describes in narrative and map form the basic land use, circulation and park concepts that give the plan its character and allow it to work. This section presents the rationale for the rules and guidelines discussed below.

Section 4. Design Guidelines: describes in narrative and illustrates in diagrams the various aspects of development (such as site planning, architecture, landscape, trails, walls and fences, etc.) which must be addressed by each development project in order to achieve the level of development quality intended by the plan.

Section 5. Public Facilities: describes the community infrastructure including water, sewer, storm drainage, flood control, and public phased services.

Section 6. Plan Implementation: includes important sections on phasing development and the necessary public improvements and services. It covers uses, site development standards and general provisions. It also describes the rules and procedures for processing development plans and, if necessary, amending the specific plan.

Section 7. Appendix: contains background data and analysis which helped in the formulation of the plan or will help in its interpretation. This material is informative, but not regulatory.

2.0 PLANNING FRAMEWORK

2.1 BACKGROUND AND PROJECT HISTORY

The proposed California Landings project site consists primarily of two individual parcels: the presently abandoned Gilfillan Air Field comprises approximately 144 acres and an abandoned chicken ranch constitutes the remaining western 64 acres. Also known as the "Old Fontana Airport", Gilfillan Airfield was at one time owned and operated by the United States Army during World War II and consisted of two runways and six ancillary buildings. In 1947, ownership of the property was eventually transferred to the County of San Bernardino which in turn leased the site for radar and electronic field testing with limited aircraft landings, and for livestock grazing. The chicken ranching operation ceased their activities in mid-1991. The chicken ranch owner removed the majority of the chicken ranch operation buildings immediately upon vacating. In addition, there are three parcels along the western boundary totaling 15 acres which the city has asked to be included in the Specific Plan. Two have an existing residence and the other one contains a metal building.

Development of the proposed California Landings project will require removal of the asphalt runways and each building. The Fontana Historic Preservation Commission has indicated that according to their criteria, the airfield is not considered a historic resource, and therefore its removal will not be considered significant since no historically significant areas exist within the California Landings Specific Plan project boundaries. Further discussion and analysis of this issue is contained within the California Landings Environmental Impact Report document.

2.2 PURPOSE

This Specific Plan has been prepared for the proposed California Landings project located in the City of Fontana. A Specific Plan is prepared for the purpose of addressing site specific issues such as development standards and aesthetics, as well as project-wide concerns such as circulation and infrastructure, in order to provide for the orderly development of the California Landings Planned Community. While a general plan examines an entire city or county, a specific plan concentrates on the development issues of a particular project. The City of Fontana's Comprehensive General Plan establish objectives which guide the preparation of this specific plan document. This ensures that the new development will meet the basic policies of environmental sensitivity, infrastructure, quality of design, and quality of planning by providing for enhanced open space, recreational, residential, and commercial uses. In this way, a specific plan provides a link between the general plan and the individual project submittals in a manner not possible with community-wide planning.

and zoning documents.

The California Landings Specific Plan establishes the vehicle of control that will implement the policies of the General Plan, by providing the type, location, and extent of land improvements and structures needed to ensure a quality development in terms of life-style opportunities, the community's built environment, and integration of the natural and human environment. Furthermore, the Specific Plan will enable the City to ensure that local service systems are consistent with development needs, efficient in design and implementation, and provided in a timely manner.

The California Landings Specific Plan provides flexibility and encourages innovative design while at the same time, setting development standards. As noted above, the California Landings Specific Plan provides the City of Fontana a complete, comprehensive and systematic approach to the implementation of the General Plan. In this regard, the California Landings Specific Plan:

- Provides distinctive residential neighborhoods with a mixture of housing types within separate residential neighborhoods.
- Creates employment opportunities through the development of commercial mixed uses.
- Incorporates a pedestrian-oriented circulation system with convenient linkages among neighborhoods, park and commercial areas to facilitate the efficient movement of people.
- Provides a large central park with a variety of active and passive recreational facilities. The park has been designed to incorporate an aviation theme, recalling the historical use of the California Landings site as an airport.
- Accommodates a comprehensive system for infrastructure and public improvements to provide adequate services in a timely manner.
- Provides quality landscaping and community monumentation that distinguish neighborhoods and create a community statement.

2.3 EXISTING AND PROPOSED LAND USES ON AND OFF-SITE

Existing On-Site Land Uses

On-site land uses within the California Landings project area consists of an abandoned asphalt airstrip, dilapidated structures and remains of an abandoned chicken ranch and manure spreading grounds located in the western portion of the site.

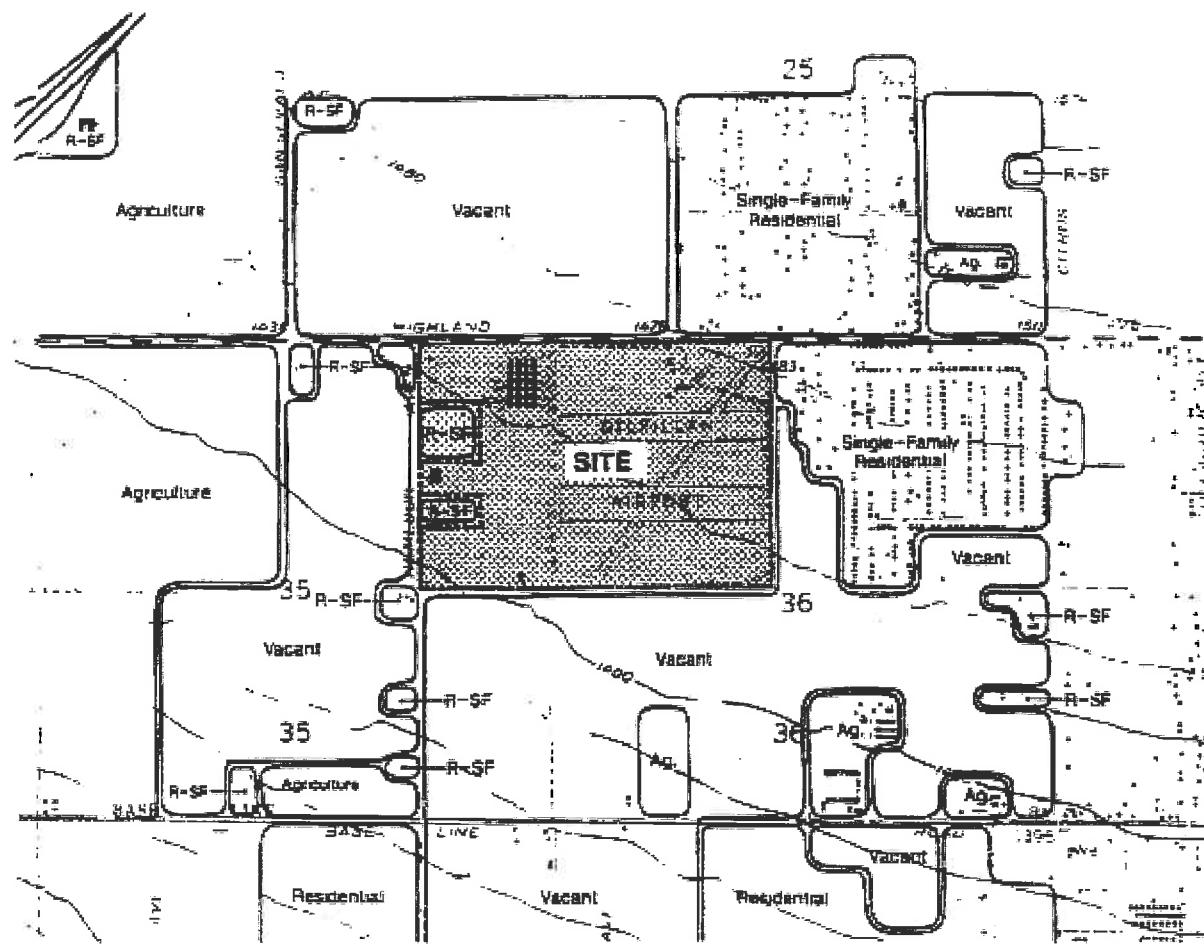
Surrounding Land Uses

The existing surrounding land uses adjacent to the site include single family residential to the east and primarily vacant property to the north, south and west (see Exhibit 4, Existing Surrounding Land Uses).

As previously noted, the California Landings Specific Plan area is located in an area generally referred to as North Fontana within the City of Fontana. In addition to the existing land uses surrounding the site, proposed land uses within the North Fontana area include the proposed Lytle Creek Specific Plan to the north, the proposed Westgate Specific Plan area to the west and the approved Rancho Fontana Specific Plan to the south as described below (see Exhibit 5, Existing and Proposed Specific Plans in the Area).

The Rancho Fontana Specific Plan has been approved for 2,295 residential dwelling units and 35 acres of mixed use and commercial development. The uses contained within the Rancho Fontana project that lies adjacent to the California Landings project site include single family homes on approximately 5,000 square foot lots.

Lytle Creek Specific Plan has a proposed residential entitlement of 792 dwelling units with 60 acres devoted to business park and commercial/office uses.



SPECIFIC PLAN AREA

R-SF

RESIDENTIAL SINGLE FAMILY

Ag

AGRICULTURE

EXISTING SURROUNDING LAND USE



ZUMI

Exhibit 4

California
LANDINGS

Kaufman & Broad ▲

Westgate Specific Plan is a proposed 870 acre project consisting of 266.5 acres of single family residential (1,117 dwelling units) and 60.5 acres of single family attached (543 dwelling units); 16.7 acres commercial; 118.5 acre business park; 15.0 acre office; 114.7 acre mixed-use; 10.0 acre elementary school; 15.5 acre park; and 95 acres of regional mixed-use.

The surrounding uses affecting the site include:

Existing Uses

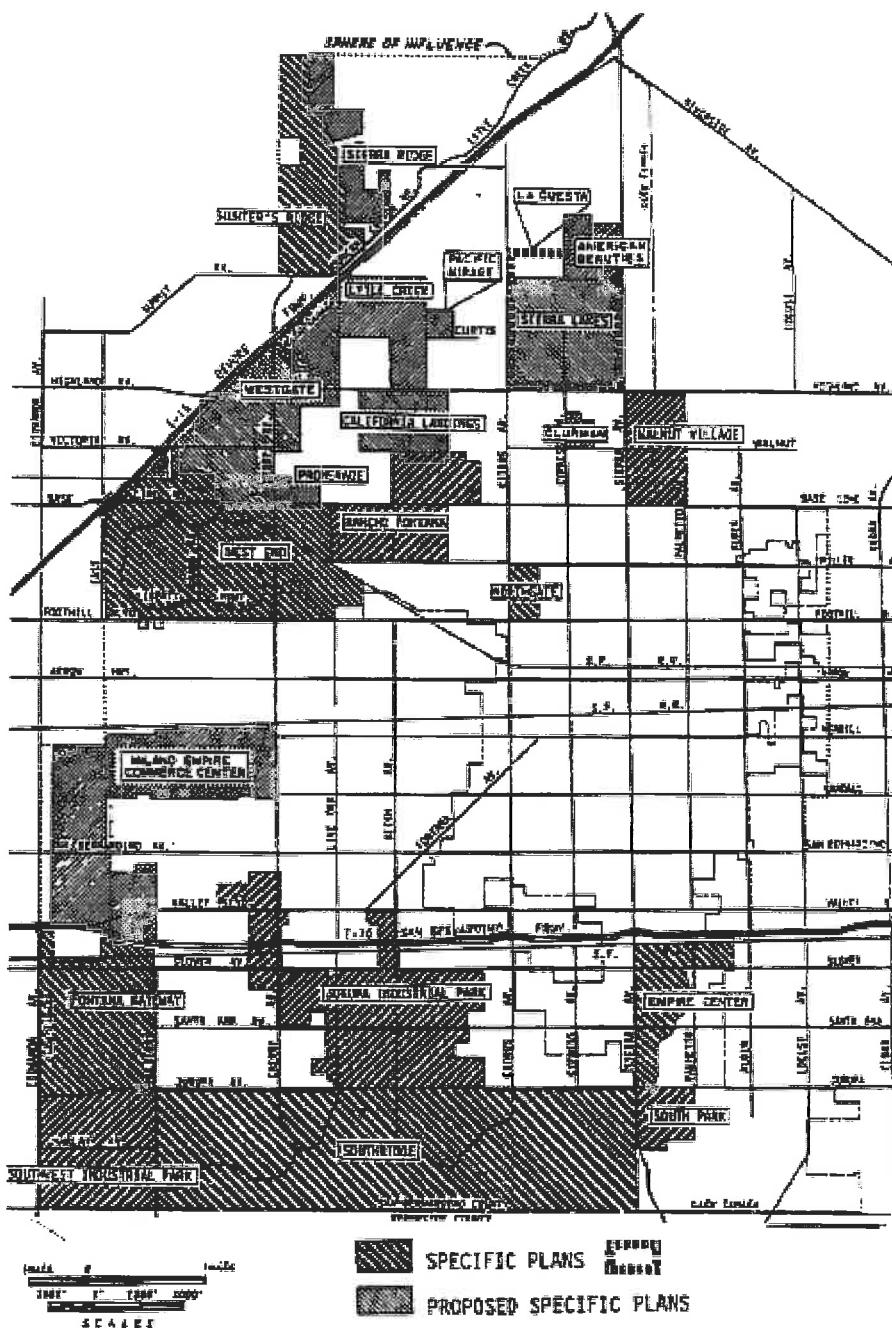
Vacant Adjacent to Project
Single Family Residential
Vacant/Agriculture

Proposed Uses

Rancho Fontana Specific Plan
Lytle Creek Specific Plan
Westgate Specific Plan

Consistent with the City's General Plan, the California Landings Specific Plan locates land uses within the site area in a pattern reflecting the existing and proposed residential communities surrounding the project while providing a northern gateway setting into the Coty. The land use categories have been located to achieve:

- Compatibility within the surrounding land use area;
- Environmental sensitivity to existing conditions;
- Appropriate interface uses along adjacent perimeters;
- Logical extension of infrastructure.



EXISTING and PROPOSED SPECIFIC PLANS IN AREA



California
LANDINGS

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2.4 GENERAL PLAN POLICY CONFORMANCE

The General Plan Land Use Map designation for the California Landings Specific Plan is R-PC, Planned Community Residential. Multi-Family Residential (R-MF), Recreational Facilities (PR) and Public Facilities (P-PF).

Residential Planned Community (R-PC)

The intent of the Residential Planned Community (R-PC) is to provide for the managed growth of master planned communities. The development criteria as contained in the City's General Plan for the R-PC area may be summarized as described below. Detailed policy compliance is analyzed in Appendix 7.2, General Plan Consistency.

1. Master Plan Requirement

All development proposals of 40 acres or more must be approved as a master planned project subject to the criteria established for Specific Plans.

Compliance: California Landings Specific Plan is prepared based on this criteria.

2. Base Density

Compliance: NA: Applies to projects not prepared under a Specific Plan.

3. Master Plan Density

The net dwelling unit range for master planned projects is 4.35 to 7.00 DU/net acre, which corresponds to a lot size range of 10,000 to 6,200 sq. ft.

Compliance: The California Landings project proposes lot sizes of 6,200 sq. ft. and 7,200 sq. ft. within the R-PC designation.

4. Minimum Lot Size

The minimum lot size for any component of an R-PC project is 6,200 sq. ft. (6,200 sq. ft. lots are allowed as part of plans with extraordinary amenities).

Compliance: California Landings is proposing to provide an extraordinary amenity to allow 6,200 sq. ft. minimum lots.

5. Active Recreation

Active recreation use open space in excess of that required by Quimby Act allocations may be 100% credited for net density calculation.

Compliance: California Landings proposes no excess active recreation area for credit.

6. Lot Size Minimum for Master Plan Projects in an R-PC Area

Lots within a master planned area must be 7,200 sq. ft. or larger (not including access easements or rights-of-way of any type; except for projects with an extraordinary open space or amenity).

Compliance: California Landings is proposing to provide 7,200 sq. ft. lots and extraordinary amenities to allow 6,200 sq. ft. minimum lots.

7. Additional requirements

In conjunction with, and in addition to, State of California Specific Plan requirements, all R-PC Specific Plans must contain the following:

A land use plan providing a mix of compatible uses incorporating high quality design concepts and a consistent community design theme.

- A circulation plan establishing appropriate access to and from the project area as well as adequate on-site circulation within the project area.
- A landscape plan providing a consistent planting theme establishing a community identity for the project area.
- Special edge treatments along major arterials and as buffers between residential and non-residential uses.

- A plan for the installation of infrastructure describing the scope and sizing of capital improvements, the level of public services such as police, fire, and schools to be provided, a phasing plan and the funding/financing mechanism proposed for construction and maintenance consistent with the infrastructure plan for the area.
- A phasing plan for the development.
- An assessment of the fiscal and economic impacts of the development.
- When determined necessary by the Director of Community Development, a development agreement shall be submitted as part of the Specific Plan to include the elements listed above and setting forth the terms and conditions agreeable to the City and the developer for implementation of the project.

Compliance: California Landings Specific Plan is prepared in conformance with this criteria.

- Land Use Plan: See Section 3.3
- Circulation Plan: See Section 3.4, and to be analyzed in project EIR
- Landscape Plan: See Section 4.4
- Edge Treatments: See Section 4.4
- Phasing Plan for development and infrastructure: See Section 6.2
- Fiscal Impacts: See Section 2.7, and to be analyzed in project EIR
- Development Agreement: to be submitted
- EIR - See EIR prepared by E.I.P. Associates, Inc.

EXISTING GENERAL PLAN DESIGNATION

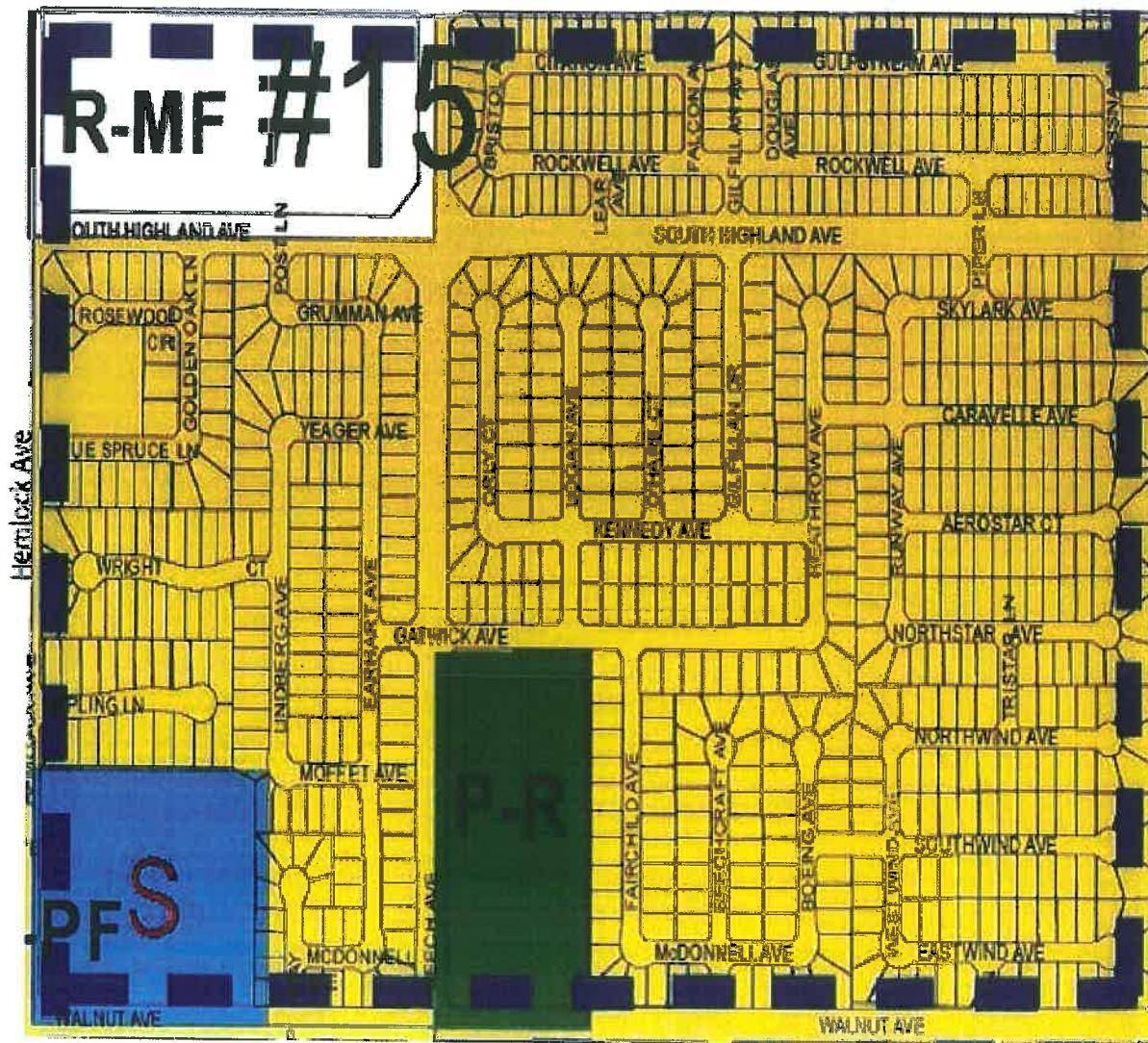


Exhibit 6

GENERAL PLAN DESIGNATION

California
LANDINGS

Multi-Family Residential (R-MF)

1. The Multi-Family Residential (12.1 – 24 du/ac) category includes multi-family developments, from duplexes and townhouses to condos at densities up to 24 dwelling units per acre with required amenities.
2. The residential component of the R-MF area must meet all of the General Plan and Zoning requirements applicable to the density of the residential development being considered.

Compliance: The California Landings project proposes 184 acres of residential use, including 14.8 acres within the Multi-Family Residential (R-MF) land use category consisting of attached and detached residential units. Apartments are not allowed within the California Landings Specific Plan. The specific uses are detailed within Section 6 of this Specific Plan and are generally similar of those specified in the General Plan.

Public Facilities (P-PF)

1. The Public Facilities (P-PF) category designation is for properties in public or quasi-public ownership, such as existing schools: the facilities of agencies such as the City, County, water and sewer districts, and fire protection districts; and hospitals and quasi-public institutions.
2. There are no residential components within the P-PF land use area.

Compliance: The California Landings project includes one (1) public school (Solorio Elementary, K-5) situated upon 9.9 acres at the northeast corner of Hemlock Ave. and Walnut Ave.

Recreational Facilities (P-R)

1. The Recreational Facilities (P-R) category designation includes regional and local parks, and any recreational facility operated by a public or quasi-public agency.
2. There are no residential components within the P-R land use area.

Compliance: The California Landings project includes one (1) public recreational facility (Koehler Park) situated upon 10 acres at the northeast corner of Beech Ave. and Walnut Ave.

2.5 GENERAL PLAN POLICY CONSISTENCY

Per City requirement, the policies of the General Plan are reviewed in relationship to the California Landings project. The findings of this review are documented in the Appendix under Section 7.2, General Plan Policy Consistency.

2.6 SUMMARY OF AMENITIES AND STANDARDS THAT EXCEED CITY CODE

The following amenities and standards proposed in this Specific Plan exceed City Standards:

2.6.1 Extraordinary Amenity

A fully dedicated and developed central park exceeding the City of Fontana fee requirement will be provided in early phases of this project with neighborhood and community park improvements.

2.6.2 Large Central Park

A large central park 10.0 acres in size will be located in the center of the project. It will have a historic theme recalling the sites previous use as Gilfillan Field and will incorporate both neighborhood and community park activities and uses. (see Section 3.5 Park and Recreation Plan).

2.6.3 Parkways

Parkways for the community road sections will be designed per the General Plan.

2.6.4 Bikeway (Off-Road)

An off-road ten (10) foot wide combined bike path and sidewalk will be provided on the north half of Walnut Avenue. This is above City standard and is in response to City request to remove the bike lane from this street and to enlarge it to be a convenient bicycle and pedestrian path.

2.6.5 Architecture and Landscape Architecture Design Guidelines

An architectural theme is established for the project entries as well as the homes, providing guidelines that exceed City standards.

A. ARCHITECTURE

In the Design Guidelines section of this Specific Plan, several architectural styles are described to encourage a richness and distinct identity with a unique "Southern California Traditional" theme. Architectural styles that will be permitted and encouraged are (see Section 4.3 for style sketches):

- Craftsman
- California Mediterranean Prairie
- Early California California
- Ranch Spanish Monterey
- Cottage
- Italian Revival

B. LANDSCAPE ARCHITECTURE

Unique landscape architectural statements will be established within the community. Several key examples are noted below in the aviation park theme and the communities upgraded entries.

2.6.6 Aviation Park Theme

An aviation park theme is proposed to provide a unique park experience recalling the historic use of the site as Gilfillan Field Airport and Weather

Station. In addition, the following park elements may be introduced in the Park (100 acres) including:

Community Park Elements

- Large covered outside group meeting area. Picnic facility areas, covered -
- with multiple barbecue and seating capabilities.
- Two lighted tennis courts.
- Two regulation size soccer and baseball fields with seating areas.
- Large Plaza - capacity for major community activities.
- Children's Play Area – 10,000 square feet - separated into age group activities and fulfilling all state handicap requirements.
- Security and Walkway Lighting - throughout the park.
- Off-street parking - 70 cars.

Neighborhood Park Elements

- Restroom Facilities.
- Two full size basketball courts.
- Volleyball court.
- Passive picnic activity areas.
- Bike racks, drinking fountains and trash receptacles.
- Informal play areas.
- Internal trail network.
- Landscaped perimeter buffer.

See Section 3.5 for park details.

2.6.7 Upgraded Entries

Entry designs included within this Specific Plan include entry monument walls and landscaped backdrops, creating a high quality, unique sense of arrival. The following entry monuments exhibit this special treatment (see sketches in Section 4.4):

- Primary Entry Monuments
- Secondary Entry Monuments
- Neighborhood Entry Monuments
- Commercial Entry Monuments

2.7 Fiscal

Fifteen (15) acres of commercial revenue generating uses will provide revenue to the City of Fontana. A complete fiscal impact analysis will be prepared as part of the Environmental Impact Report (E.I.R.) for the project.

3.0 COMMUNITY DEVELOPMENT

3.1 INTRODUCTION

The California Landings Specific Plan is a master planned community that includes residential neighborhoods, commercial, and recreational park linked by an efficient circulation network. The California Landings project is planned to function within the City's development framework, while still maintaining an individual identity. To ensure this, a number of design objectives were established in conjunction with major land use, circulation, and park components of the development plan. The purpose of this section is to delineate the overall framework of the California Landings community through the establishment of project-wide design objectives and land use, circulation, and park development plans.

3.2 PLAN CONCEPT AND DESIGN OBJECTIVES

Plan Concept

The primary plan concept and design objective for the California Landings Specific Plan is to create a quality, family-oriented community which is pedestrian oriented, and provides a long-term, self-contained neighborhood including housing, jobs, services, recreation and public facilities.

The California Landings Specific Plan is designed to facilitate:

1. The logical and orderly development of a balanced community that is compatible with surrounding land uses, and that is consistent with the land use policies of the City's General Plan;
2. The provision of a cohesive community design theme that promotes complementary streetscape elements and high quality architectural styles to create an aesthetically pleasing community;
3. The provision of a variety of housing opportunities and residential life styles within the City;

4. The development of and dedication of public areas for community recreational uses;
5. Implementation of public facilities and services in a timely manner; in conjunction with City of Fontana and County of San Bernardino Master Plans.

Design Objectives

The design objectives of the California Landings Specific Plan are to provide a responsive plan which enhances the characteristics of the site and surrounding area. The Specific Plan emphasizes the goal of compatibility with surrounding community land uses, while promoting a quality development and aesthetically pleasing living environment. The California Landings design theme will be reinforced by the following design and community elements:

- Creation of a variety of residential product types to create distinct neighborhoods.
- Implementation of a cohesive circulation system through the consistent street design serving internal and external uses.
- Utilization of the community circulation network as a means to link individual project neighborhoods.
- Provide the opportunity for an elementary school on-site to meet the educational needs of the children of the community.
- Installation of streetscape elements and community entry monumentation to create a unique community image.
- Creation of a safe pedestrian and bicycle oriented streetscape which residents can utilize for recreational purposes.
- Provision of an aesthetic and functional buffer between vehicular areas and residential/commercial land uses, as well as adjoining properties.
- A large neighborhood park will be provided with many community park amenity items which may include:
 - Small Activities Building
 - Cabana
 - Sports Facilities, such as full-size soccer, football or

baseball fields

- Tennis and/or Basketball Court(s) - full size
- Outdoor Handball/Racquetball Courts
- Play Tot Lot
- Group Picnic Area
- Barbecue Area and Picnic Tables

- A community trail system for bicycle and pedestrians connecting residential neighborhoods with the park and commercial center.
- An aviation theme for the park recalling the historic use of the Landings site as an airfield and providing a unique park design.
- Landscape parkways along all secondary and major roadways. Entry themes to create a sense of "community" and arrival.
- Commercial land uses providing needed and desired services creating a balanced community.
- Pedestrian orientation with parkways and access to the park, commercial and residential areas.

Other Objectives:

With respect to implementation and future approvals, the California Landings Specific Plan further achieves a variety of administrative objectives, including:

- Provision of a comprehensive systematic approach to the implementation of the General Plan;
- A means to evaluate the Plan area in detail and set development standards to enhance the subsequent review of the tentative maps; and
- Providing City Staff with a document for zoning, development standards, public facilities implementation, phasing, and circulation needs.

Development Plan

The General Plan Land Use Map designation for the California Landings Specific Plan area includes Planned Community Residential (R-PC) and Community Mixed Use (CMU) which provides for a mix of residential housing types. The California Landings Specific Plan implements the existing General Plan land uses for the site by consolidating development currently permitted throughout the property into various residential neighborhoods. Furthermore, the Specific Plan incorporates development standards to provide for implementation of the proposed land uses consistent with the City's General Plan. In accordance with the requirements of the City's Specific Plan Ordinance, these standards are patterned after the zone districts contained in Chapter 30 of the City of Fontana Zoning Code. In general, the development standards for the project provide the mechanism for increasing open space, minimizing grading and enhancing innovative land use planning techniques by addressing the unique characteristics of the California Landings site.

3.3 LAND USE

The purpose of the land use element of the California Landings Specific Plan is to establish a comprehensive development plan for the proposed project. The California Landings planned community includes 985 dwelling units (41 of which are located on property owned by others, 10 acres of park, and approximately 18 acres of major roads on a 223 acre site (other ownership includes approximately 15 acres). The community will be unified through the use of design consistency and coordinated entry statements, landscaping, architecture, and signage.

Exhibit 7 depicts the Comprehensive Land Use Plan and Table 3.1 summarizes the project statistics by land use designation and neighborhood.

The California Landings Specific Plan development consists of 11 planning areas including residential, park and school uses. Eight neighborhoods are proposed as residential land uses (2 of which represent other ownership), along with one (1) planning area for park use and one (1) planning area for an elementary school.

These land uses are described below, and specific design guideline information for each of the planning areas within California Landings is provided in Section 4.0, Design Guidelines.

A. RESIDENTIAL - The housing mix is proposed in different density ranges.

- Single Family Detached (SFD) – This designation will consist of approximately 126 acres on site. Minimum lot sizes will be 6,200 – 7,200 square feet. These dwelling units will be located in neighborhoods A, B, C, D and E. One large central park will be located within these neighborhoods to provide convenient access to residential Single Family Detached residential on 10,000 sq. ft. lots is also proposed in the Other Ownership Neighborhoods 1 and 2.
- Single Family Detached (SFD, 5,000 sf min) – This designation will consist of approximately 29.7 acres. Minimum lot size will be 5,000 square feet. These dwelling units will be located in Neighborhood F.
- Detached & Attached Cluster Residential – This designation will consist of approximately 14.8 acres. There is no minimum lot size. Housing type will be attached and detached cluster residential units.

B. PARK - The California Landings planned community will offer 10.0 acres of land for a large central park with recreational amenities. The Quimby Ordinance of the City of Fontana permits that 3 acres of (neighborhood) parkland be provided for every 1,000 persons in population. The California Landings project (based on Park Development Ordinance 1061 occupancy rates) requires 13.05 acres of park for the Kaufman & Broad Specific Plan parcels. Park acreage is met by providing the following: (see Section 3.5 Park and Recreation Plan for further detail).

Kaufman & Broad

The Kaufman & Broad ownership is 208 acres with 750 units of the 791 homes. This portion of the project generates 2,610 people and requires 7.8 acres of neighborhood park on site. This park acreage is exceeded by providing a total of 100 acres as follows:

- Large Central Park: A 10.0 acre aviation theme park is proposed. The facility will provide a major recreational opportunity for the community. Park land dedication and improvements will be installed by the applicant.

- Bike Trails: Within California Landings Walnut Avenue will have bicycle and pedestrian trails. This bike trail will provide an attractive, functional and convenient bike trail to unify and link the land uses and neighborhoods together.

Other Ownership

The other ownership areas (41 du) were not included in Quimby calculations at the request of the City Park Planning and Development Coordinator. This land will be subject to in- lieu fees or park dedication in the future in accordance with Quimby requirements.

LAND USE PLAN

AMENITY ALTERNATIVE

STATISTICAL SUMMARY

LAND USE KAUFMAN & BROAD	NET AC	DU
RESIDENTIAL SINGLE FAMILY DETACHED (SFD)		
NEIGHBORHOOD:		
A 7,200 S.F.	2.0	98
B 6,600 S.F.	1.5	135
C 6,200 S.F.	1.7	53
D 6,200 S.F.	1.6	91
E 6,200 S.F.	1.6	218
F 5,000 S.F.	2.7	165
RESIDENTIAL SUBTOTAL	156.7	750
DETACHED & ATTACHED CLUSTER RESIDENTIAL	14.8	134
ELEMENTARY SCHOOL	10.0	
PARK	10.0	
CIRCULATION	17.7	
PROJECT TOTAL	208.2	944
OTHER OWNERSHIPS		
RESIDENTIAL, SINGLE FAMILY DETACHED (SFD)		
NEIGHBORHOOD:		
1 10,000 S.F.	8.7	
2 10,000 S.F.	4.6	
RESIDENTIAL SUBTOTAL	13.5	41
CIRCULATION	1.5	
OTHER OWNERSHIP TOTAL	15.0	41
SPECIFIC PLAN TOTAL	223.2	985
LEGEND	—	—

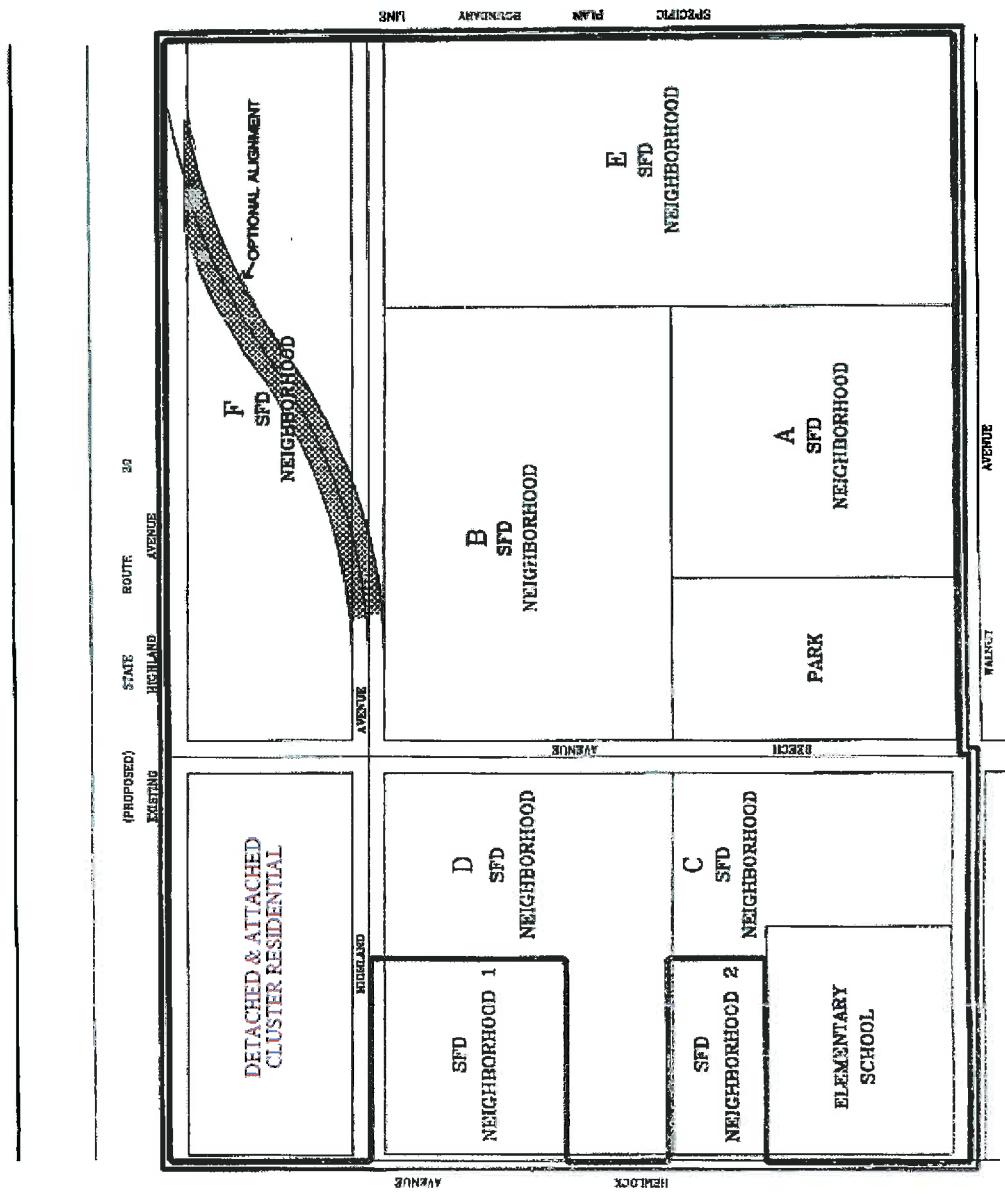


Exhibit 7

California
LANDINGS

Kaufman & Broad

3-7



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TABLE 3.1
LAND USE PLAN SUMMARY

LAND USE	ACRES	DU	NET DENSITY (DU/ACRE)	% OF SITE
<u>KAUFMAN & BROAD</u>				
RESIDENTIAL				
Single Family Detached (A-F)	155.7	750	4.8	69.7%
DETACHED & ATTACHED CLUSTER RESIDENTIAL	14.8	--	--	6.6%
ELEMENTARY	10.0	--	--	4.5%
PARK	10.0	--	--	4.5%
CIRCULATION	<u>17.7</u>	--	--	<u>7.9%</u>
SUBTOTAL	208.2	750	--	93.2%
<u>OTHER OWNERSHIP</u>				
RESIDENTIAL				
Single Family Detached (1 & 2)	13.5	41	3.0	6.1%
CIRCULATION	<u>1.5</u>	--	--	<u>0.7%</u>
SUBTOTAL	15.0	41	--	6.8%
TOTAL	223.2	791	--	100.0%

3.4 CIRCULATION PLAN AND ROAD SECTIONS

The purpose of the circulation element of the Specific Plan is to establish the general layout and design standards for roadways in the California Landings project area and to implement the Circulation Element of the City of Fontana. The Circulation Plan proposes a roadway and trails system which extends existing circulation and creates efficient circulation routes through the site (see Circulation Plan, Exhibit 8). The main objective of the Circulation Plan is to provide direct and convenient access to residential, commercial and recreational land uses through a safe and efficient network including major and local collectors. Proposed typical street sections are shown on Exhibits 9, 10, 11 and 11a.

The circulation system for the California Landings Specific Plan includes the following major elements:

ROADWAYS

The project will be served by existing Highland Avenue to the north, Hemlock Avenue to the west and by the proposed extensions of Walnut Avenue on the south and Beech Avenue passing in a north/south orientation through the project site. The project site is adjacent to and just south of the proposed Foothill Freeway (State Route 30) corridor and may be served through the proposed Beech Avenue High Occupancy Vehicle (H.O.V.) off-ramp.

Highland Avenue (State Route 30) - is an existing two-lane roadway (60' R.O.W.) and is designated as a key east-west roadway by the City of Fontana. Highland Avenue presently borders the entire northern portion of the California Landings property. Highland Avenue may ultimately be improved to a primary highway, with four lanes and a median, within a 104 foot right-of-way, and realigned through the site to accommodate off-ramps for the proposed Route 30 (Foothill Freeway Corridor). The street section provided in Exhibit 9 depicts the future 104' right-of-way for Highland Avenue. A 14' center median is shown bordered by 2 travel lanes totaling 33' each (14' wide lane against median and 19' outside lane with emergency parking provisions are proposed). On-street Class II bikeways are also proposed. Two 12' parkways including 6' walkways, are proposed on both sides of the street.

Beech Avenue - is proposed as a north-south four-lane divided roadway. Beech Avenue will bisect the property and will be improved as a primary highway within a 104 foot wide right-of-way (see Exhibit 9). On-street Class II bike lanes are also proposed. Two (2) 12' parkways with 6' walkways are proposed on both sides of the street.

Walnut Avenue - The proposed western extension of Walnut Avenue would be a two-lane divided collector roadway bordering a portion of the southern side of the California Landings property. Walnut Avenue will be improved as an 86 foot wide right-of-way containing a pedestrian/bike trail (see Exhibit 10).

Hemlock Avenue - Hemlock Avenue is an existing two-lane undivided roadway which borders the California Landings property on its western boundary. Hemlock Avenue will be improved as a collector road within a 68 foot wide right-of-way, of which the western 20 feet will be improved by the adjacent owner/developer (see Exhibit 10).

Future Route 30 - A High Occupancy Vehicle (H.O.V.) interchange is proposed for Beech Avenue. The vertical and horizontal alignments are not finalized and may slightly impact ultimate size of commercial parcels.

Internal Roadways - As shown on Exhibit 8, Circulation Plan, 17.7 acres of the project site would be set aside for construction of proposed internal roadways. Local street connections to the two out-parcels will be provided for future access. As shown in Exhibit 11, local streets will have a 60-64' right-of-way.

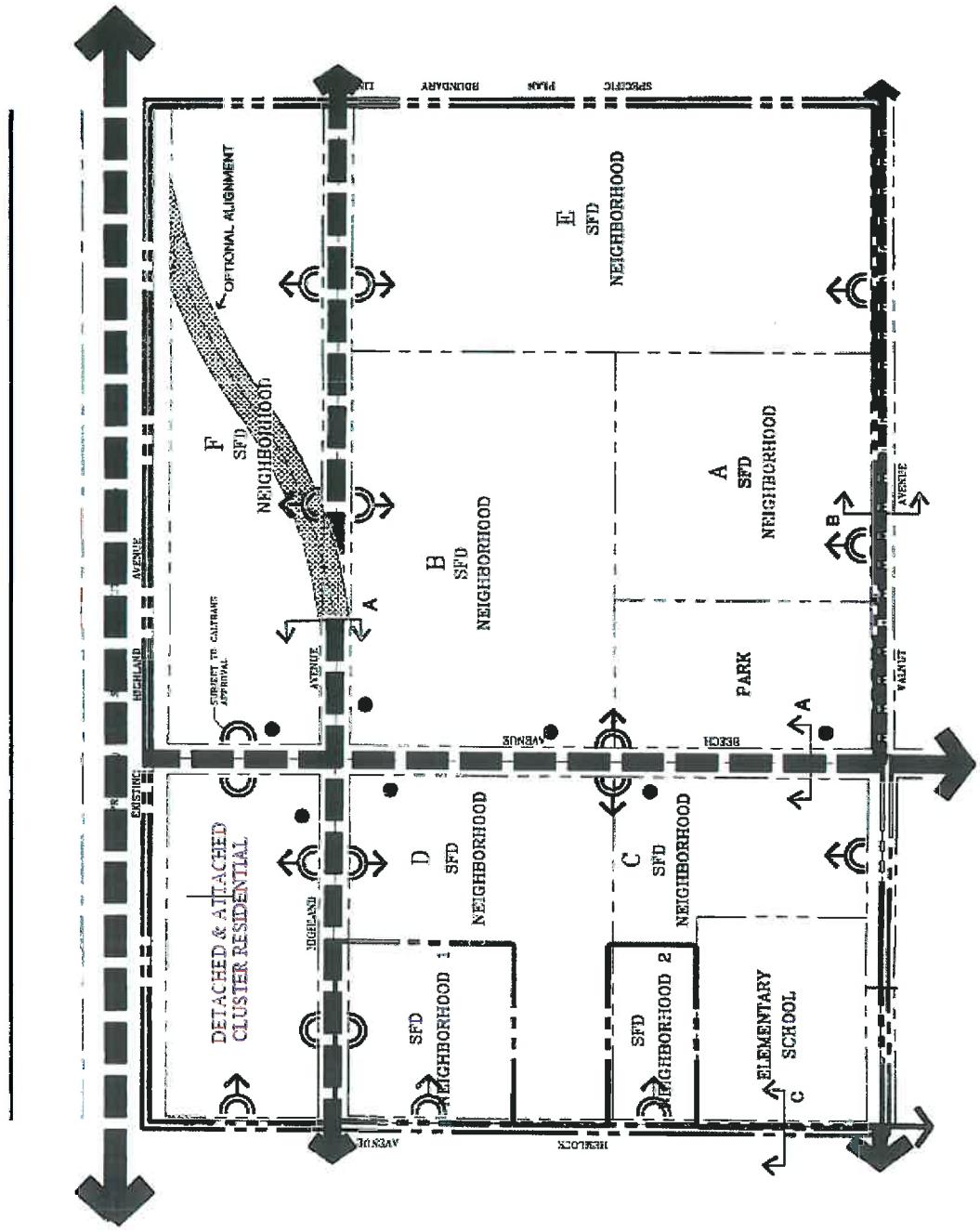
Private streets within the attached and detached cluster residential area shall have minimum curb-to-curb widths of 26 ft.

Further discussion of the planning and implementation of the proposed circulation system can be found in the Plan Implementation, Section 6.0.

CIRCULATION PLAN

LEGEND:

- PROPOSED ROUTE 30
- A. PRIMARY HIGHWAY^o
(104' R.O.W.)
- B. DIVIDED COLLECTOR
(86' R.O.W.)
- C. COLLECTOR STREET
(68' R.O.W.)

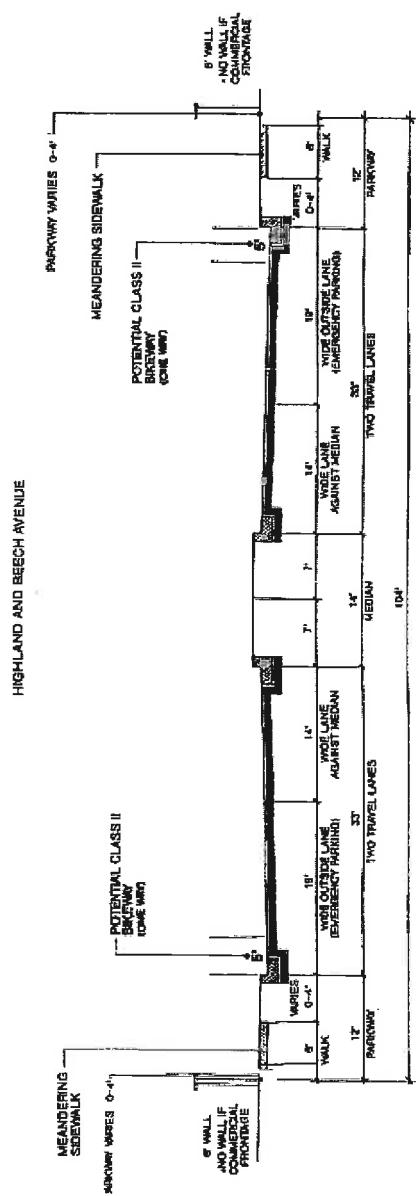


California
LANDINGS

Kaufman & Broad

Exhibit 8

3-11



SECTION 'A' - 104' R.O.W.
PRIMARY HIGHWAY
 (BEECH AVENUE AND HIGHLAND AVENUE)

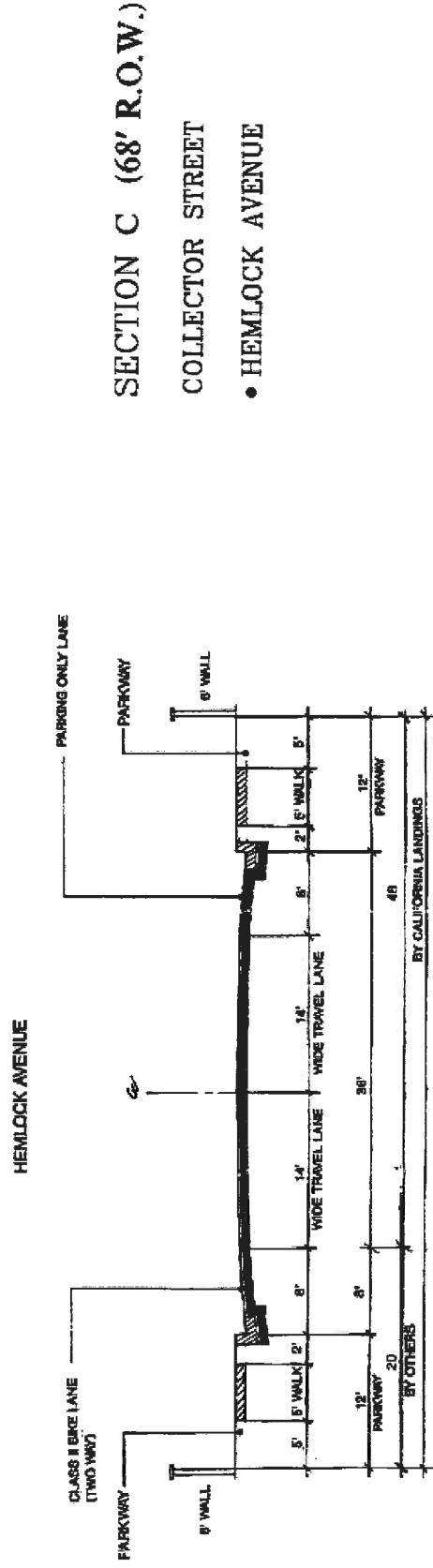
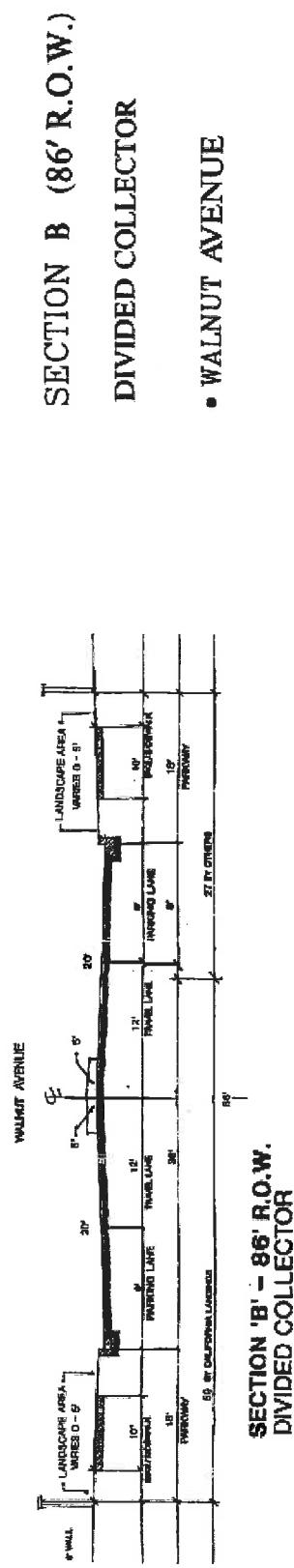
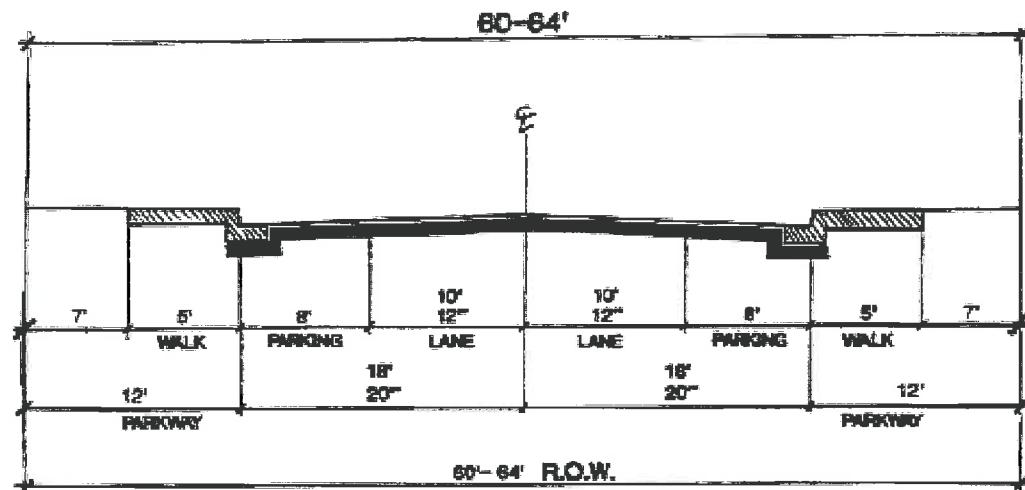


Exhibit 10
Kaufman Broad

California
LANDINGS

12/24



SECTION 'G' -60' WIDTH

LOCAL STREET

*SHORT LOCAL STREET 60' (600' OR LESS)

*LONG LOCAL STREET 64' (MORE THAN 600')

SECTION G (60-64' WIDTH)

LOCAL STREET

Exhibit 11

California
LANDINGS

3.5 PARK AND RECREATION PLAN

The California Landings Specific Plan provides a total of 10.0 acres of park, bike trails and open space to meet the recreational needs of future Landings residents (see Park Plan, Exhibit 12). The park will be dedicated and improved by the applicant per the specific plan.

3.5.1 Park

The large central park site is located in the central portion of the property and is 10.0 acres in size (see Gilfillan Park Illustrative Plan, Exhibit 13). The historical use of the site as Gilfillan Field and Weather Station will be incorporated into the theme elements in the park. Control tower monumentation, park dedication plaque and the use of aviation play equipment in the tot lot area is an example of the elements that will be used to strengthen the historic theme (See Exhibits).

Community Park Elements

- Large covered outside group meeting area. Three group picnic facility areas, covered – with multiple barbecue and seating capabilities.
- Two lighted tennis courts.
- Two regulation size soccer and baseball fields with seating areas.
- Large Plaza - capacity for major community activities.
- Children's Play Area - 10,000 square feet - separated into age group activities and fulfilling all state handicap requirements.
- Security and Walkway Lighting - throughout the park.
- Off-street parking - approximately 70 cars

Neighborhood Park Elements

- Restroom Facilities.
- Two full size basketball courts. Volleyball court.
- Passive picnic activity areas.
- Bike racks, drinking fountains and trash receptacles. Informal play areas.
- Internal trail network.

- Landscaped perimeter buffer.

3.5.2 Pedestrian/Bike Trails

The park site, the residential and commercial land uses within the project are linked together by a Pedestrian/ Bikeway Trail and a greenbelt system within the parkways of Beech Avenue, Walnut Avenue, and Highland Avenue (refer to Exhibit 40). Implementation of this trail system is intended to incorporate landscaping and a pedestrian trail, consistent with the City's proposed Residential Planned Community General Plan designation, and the City's Hiking/Bike Trail illustration within the proposed Open Space and Recreation Element. Bike racks shall be provided at the park site.

3.5.3 Other Ownership

The other ownership parcels (see Appendix, Section 7.1) will mitigate their park requirements by paying the adopted city fee for parks at the time of building permit issuance.

3.5.4 City and Quimby Act Parkland Requirements

With respect to park requirements, the California Landings Specific Plan intends to provide a combination of land dedication and improvements to fulfill the City of Fontana Community Development Department / Engineering Division - Park and Landscape Development requirements and the State of California Quimby Act (Section 6647: California State Subdivision Map Act).

The project is required by General Plan Policy 11.2.6 to provide 3 acres of land per 1,000 persons for neighborhood parks, and 2 acres of land per 1,000 persons for community parks. California Landings will fulfill both neighborhood and community park requirements in accordance with city policies through on-site land dedication and construction of a large central park that includes community park features.

California Landings will calculate its park requirements based on the following definitions in the City of Fontana:

- Dwelling Type: Single Family Detached
- Number of Units: 750 units
- Persons Per Unit: 3.48 Persons in SFD Units (Ordinance 1061)
- Population = (Units) x (Persons per Units)
$$(750) \times (3.48) = 2,610 \text{ Population}$$

Park Requirements:

- a. **Neighborhoods Parks:** California Landings must provide neighborhood park based on Quimby Act and General Plan Policy 11.2.6 requirements:
 - $(3 \text{ acres}) \times (2,610 \text{ population}) + 1,000 = 7.8 \text{ Ac.}$
- b. **Community Park:** California Landings must provide community park based on Quimby Act and General Plan Policy 11.2.6 requirements:
 - $(2 \text{ acres}) \times (2,610 \text{ population}) + 1,000 = 5.2 \text{ Ac.}$
- c. **Total Neighborhood and Community Park:** Based on the above, California Landings is required to provide 13.0 acres of neighborhood and community parkland.
 - $7.8 \text{ Ac.} + 5.2 \text{ Ac.} = 13.0 \text{ Ac.}$

Project Park Dedication: California Landings proposes 10 acres of neighborhood park. This exceeds the neighborhood park requirement of 7.8 acres. California Landings will apply the excess neighborhood park credit towards its community park requirements

Park Type	Landowner Dedication Provided	City Dedication Required	Excess Acreage	Difference
Neighborhood	10.0 ac.	7.8 ac.	2.2 ac.	
Community	2.1 ac.	5.2 ac.	0.0 ac.	3.0 ac.

The 3.0 acreages difference will be provided through enhanced community park features and facilities that will be completed in early phases of the project.

PARK PLAN

LEGEND:

100

100

TOTAL

Exhibit 12

California LANDINGS

Kaufmann Broad

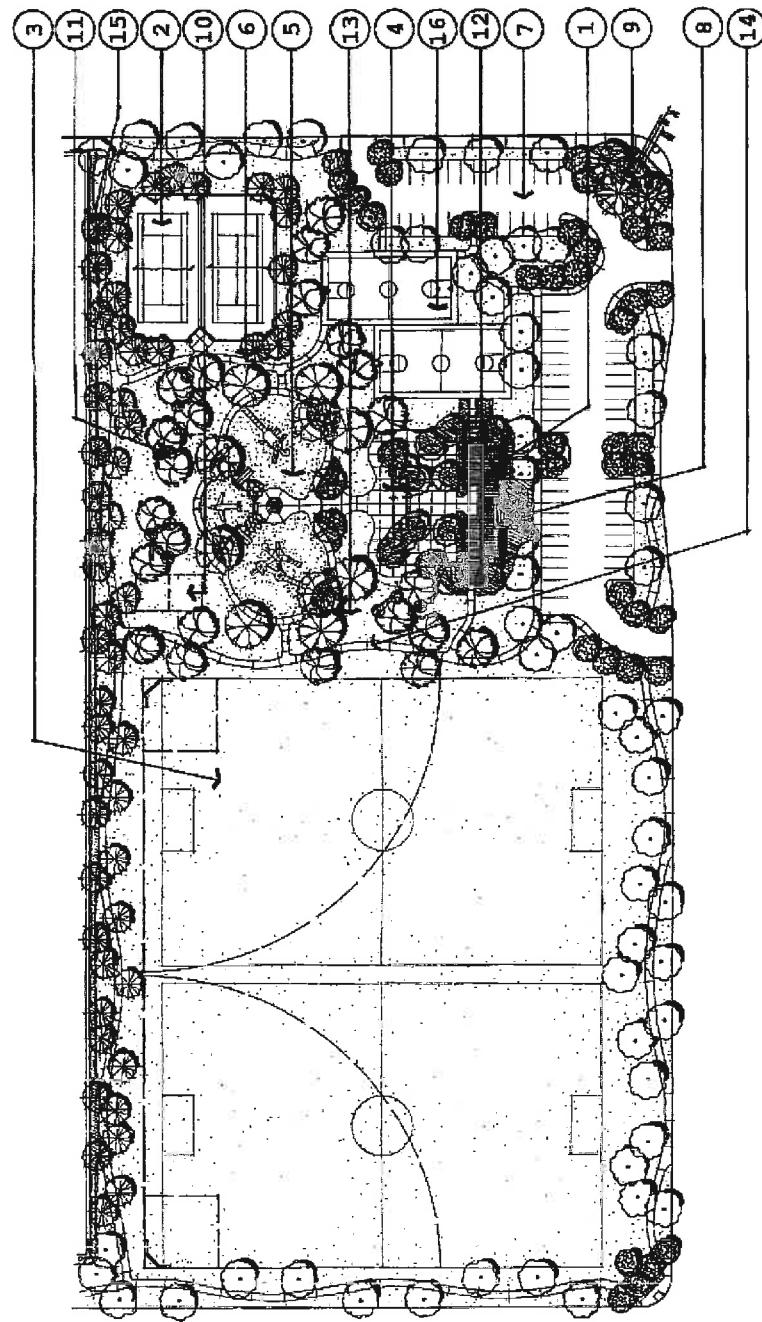
3-18

卷之三



GILFILLAN FIELD PARK

ILLUSTRATIVE PLAN (10.0 ACRES)



(FOR ILLUSTRATIVE PURPOSES ONLY)

Exhibit 13

Cajun
LANDINGS

EXHIBIT LEFT BLANK
INTENTIONALLY

Exhibit 14.

Kaufman
LANDINGS

Kaufman Δ Broad

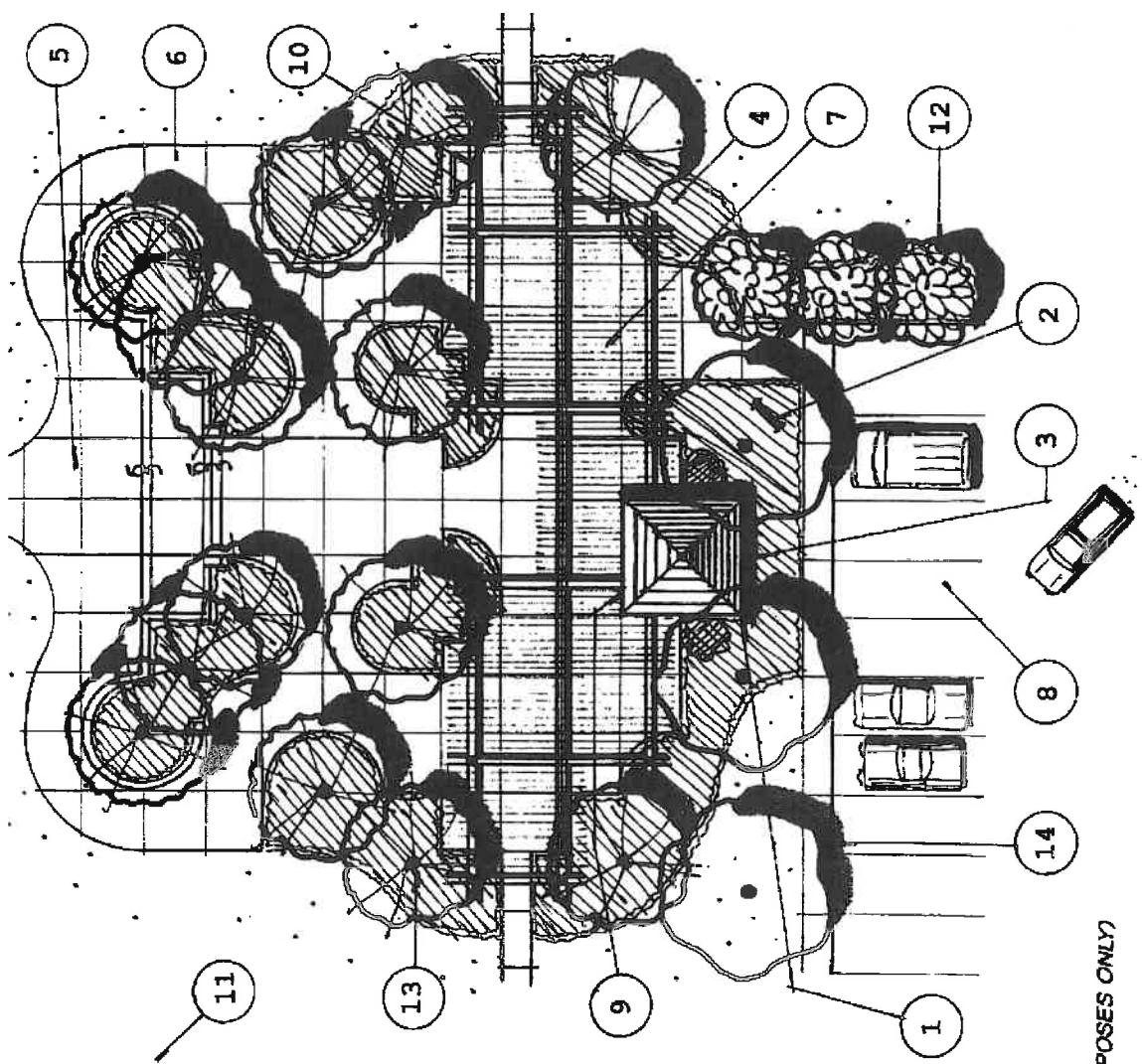
3-20

 **KAUFMAN**
LANDINGS

PEDESTRIAN PLAZA WITH PICNIC PAVILLION

LEGEND:

1. THEMATIC GILFILLAN AIRFIELD RESTROOM FACILITY.
2. LOW ENTRY SIGN.
3. GILFILLAN AIRFIELD BRASS LOGO PLAQUE.
4. ACCENT SHRUBS AND GROUND COVER.
5. LARGE MULTI-LEVEL PEDESTRIAN PLAZA AND STAGING AREA FOR COMMUNITY ACTIVITIES.
6. ENHANCED PAVING.
7. LARGE WOOD SHADE TRELLIS.
8. PARKING AREA.
9. DRINKING FOUNTAIN.
10. BIKE RACKS.
11. PICNIC TABLES AND BBQ'S LOCATED UNDER LARGE SHADE TREES.
12. SMALL FLOWERING ACCENT TREE.
13. LARGE FLOWERING CANOPY TREE.
14. SIDEWALK.



(FOR ILLUSTRATIVE PURPOSES ONLY)

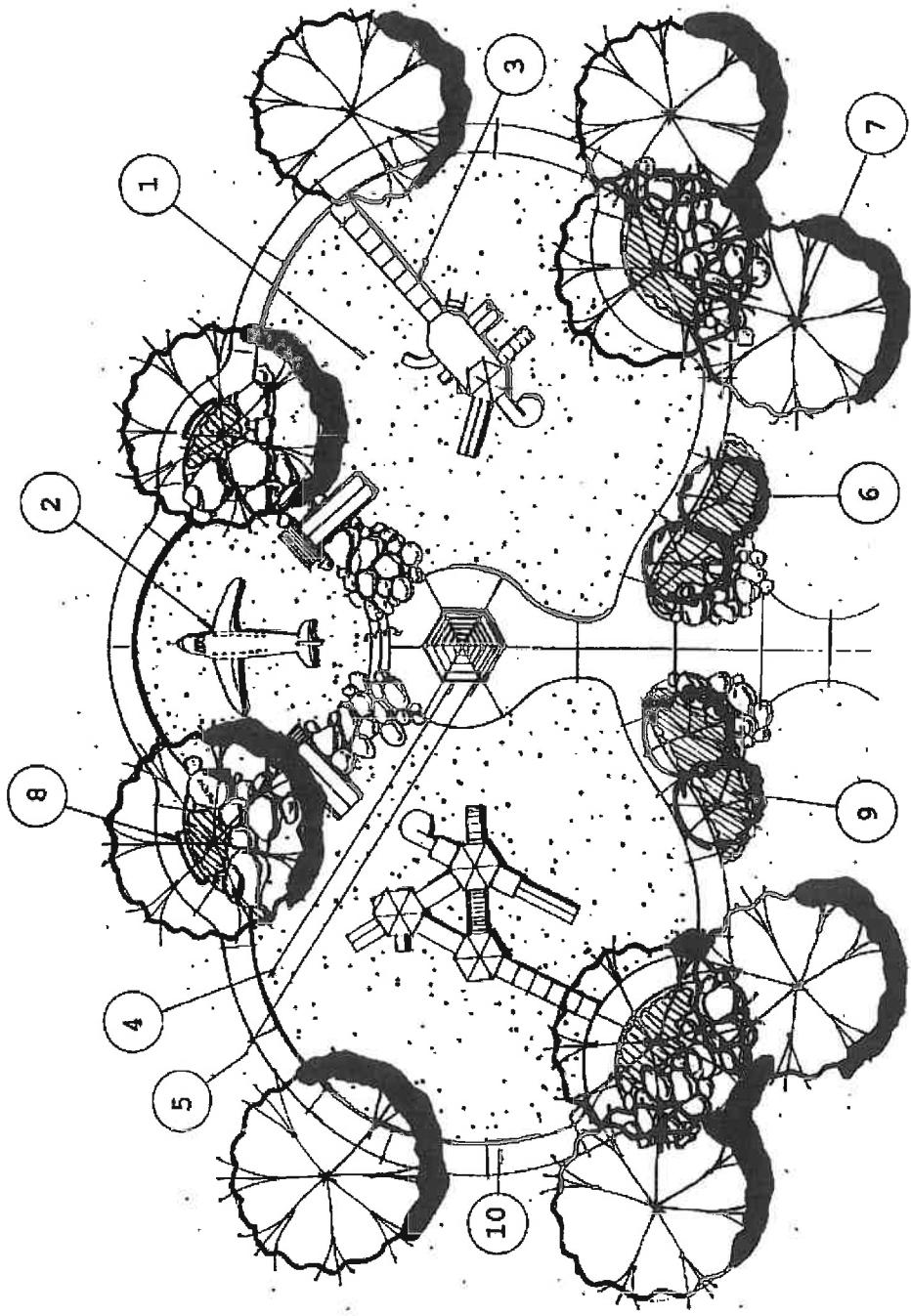
Exhibit 15

California
LANDINGS

PARK TOT LOT

LEGEND:

1. 10,000 SF CHILDRENS PLAY AREA.
2. THEMATIC PLAY EQUIPMENT WITH MULTIPLE LEVELS, SEPARATE USER GROUP AREAS.
3. HANDICAP ACCESSABLE PLAY EQUIPMENT.
4. GROUP PICNIC PAVILLION WITH TOT LOT OBSERVATION AREA.
5. DRINKING FOUNTAIN.
6. SMALL FLOWERING ACCENT TREE.
7. LARGE SHADE TREES.
8. BENCHES AT TOT LOT PERIMETER.
9. ACCENT SHRUBS AND GROUND COVER.
10. SIDEWALK.



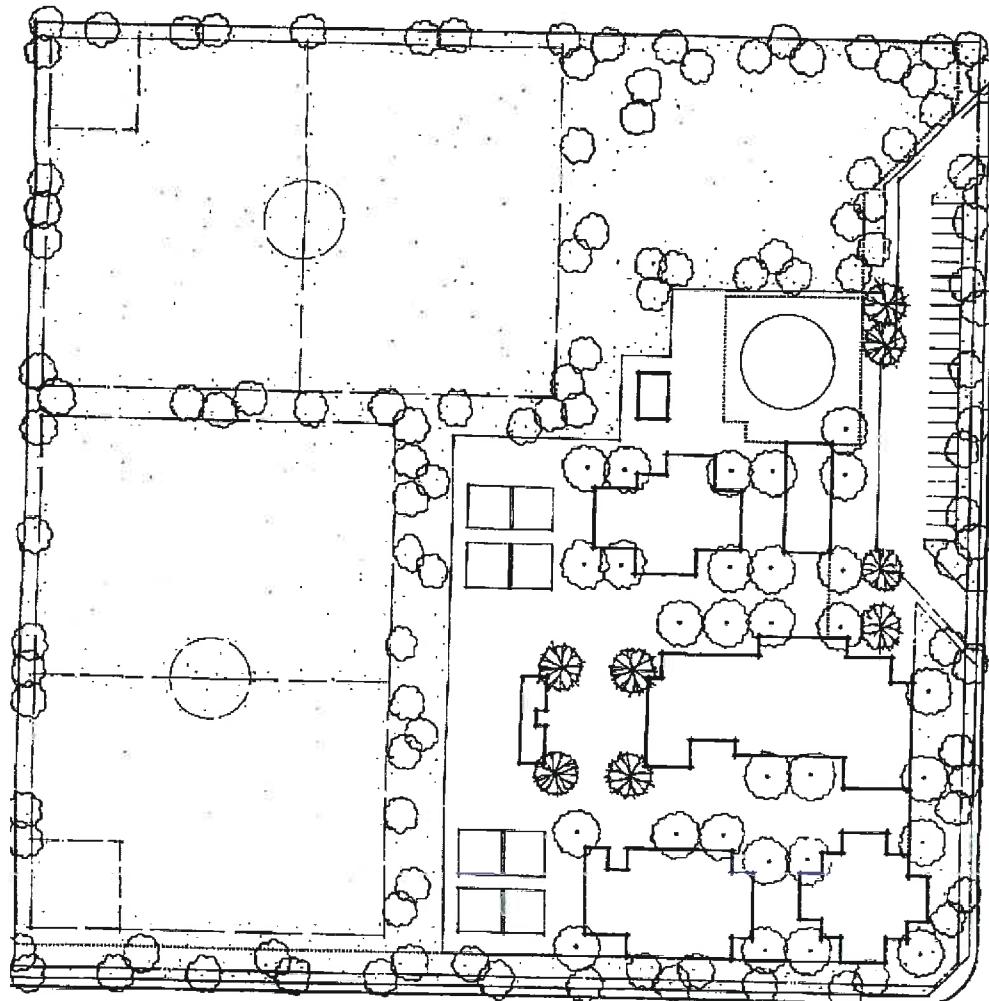
(FOR ILLUSTRATIVE PURPOSES ONLY)

Exhibit 16

California
LANDINGS

SCHOOL SITE
AMENITY ALTERNATIVE

Exhibit 17
California
LANDINGS
Kaufman & Broad



ILLUSTRATIVE ONLY



3-23

4.0 DESIGN GUIDELINES

4.1 INTRODUCTION

The purpose of these guidelines is to provide site planning and architectural design criteria that will promote a quality development and an aesthetically pleasing living environment within the Specific Plan area. These guidelines, when combined with the landscape guidelines presented in this Section, provide the overall framework for establishing a sense of community identity, land use character, scale and environmental sensitivity in the planning and design of development within the Specific Plan area.

The following guidelines express "intent" rather than "absolutes", thereby allowing flexibility in fulfilling the intended design goals and objectives. Moreover, these guidelines are intended to promote creativity and innovation as well as consistent quality in the implementation of land uses. The development design guidelines accompany the California Landings Specific Plan for the purposes of providing guidance and continuity, and to assure the implementation of development compatible with the products and applications.

4.2 SITE PLANNING

4.2.1 Guidelines

- Buildings and other improvements should be located to complement that portion of the site on which they are to be placed.
- One and two-story homes should be integrated to create variation and a sense of scale.
- Attention should be given to the design and placement of dwellings with regard to the preservation of privacy, maximum utilization of open space, natural and man-made slopes and grade differentials, and the location of visual amenities.
- Local street system design to consider ease of auto traffic movement and to provide opportunities for pedestrian, bicycle and other alternative transportation modes.
- Local street layouts should promote compatibility with building sites

and lot arrangements to endure consistent streetscape character.

4.3 ARCHITECTURE

4.3.1 Introduction

The purpose and intent of the architectural guidelines for California Landings is to encourage design that will have a distinct identity while expressing a thoughtful integration of building structures in a quality, master-planned community. These guidelines will provide the City of Fontana with the necessary assurances that this community will develop in accordance with the quality and character proposed herein. To that end, these guidelines provide guidance for City staff and the Planning Commission for evaluating the architectural design of projects within the specific plan.

The primary goal of these guidelines is to achieve a quality neighborhood environment comprised of a variety of architectural styles that will fit well together. The architectural guidelines are intended to convey a feeling, or impression, rather than have a particular style stand out.

Southern California Traditional architecture will characterize the residential environment of California landings/ The primary entry monumentation (see Exhibit 36) establishes this Southern California traditional style, borrowing design elements from California Craftsman architecture. This character will be continued through the secondary neighborhood, community wall, signage, and project walls, as well as neighborhood park. The use of stone as a timeless and trendless material adds a sense of permanence that is carried throughout the project.

4.3.2 Architectural Styles

In an effort to promote richness with the diversity of architectural styles, a unique Southern California style will be the theme. While diversity is encouraged, chaos is not, therefore, several styles will be permitted, but some styles will be prohibited. Architectural styles that will be allowed and encouraged are: Craftsman, California Mediterranean, Prairie, Early California, California Ranch, Spanish Monterey, Cottage and Italian Revival. Styles that specifically will not be allowed are: Victorian, French Country and Tudor. Each home has its own character identity. These will not be designed with trends that come and go with the shims of the marketplace, but with styles that have been long lasting and that make up the Southern California look. We call this design “Southern California Traditional” architecture; borrowing from the

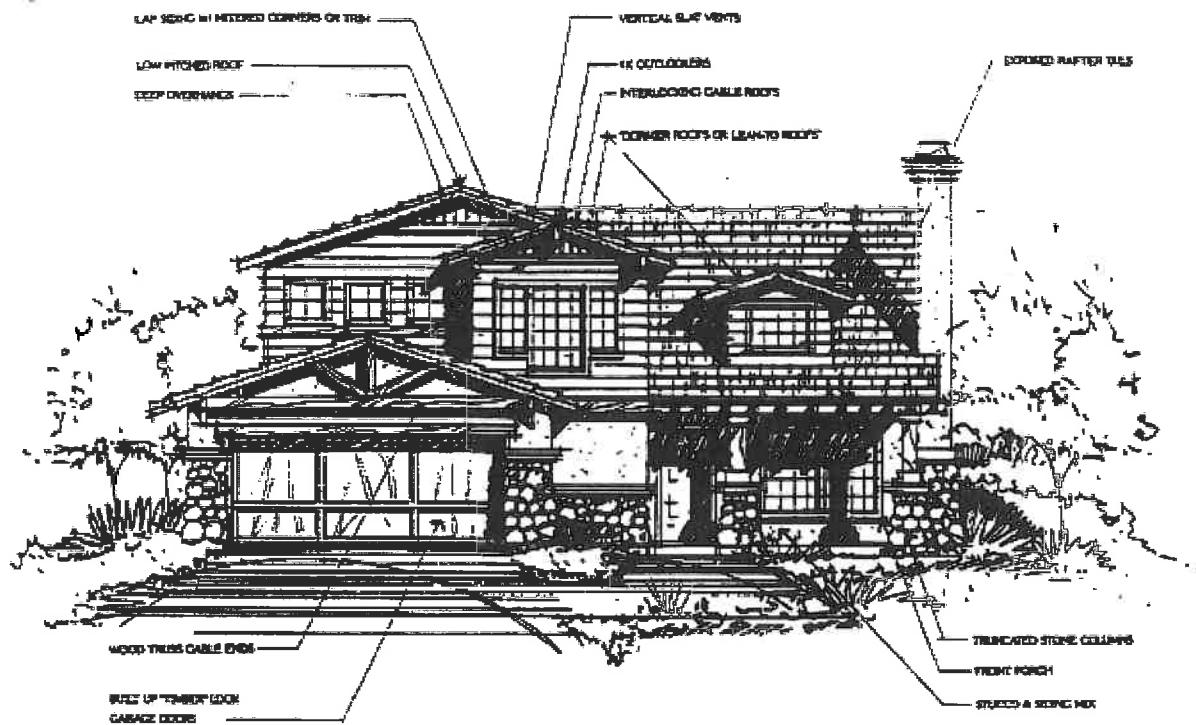
elements of traditional California architecture.

The examples of the architectural styles that are presented are to provide background for guidance and inspiration in creating this image. Contemporary interpretations of these styles are encouraged, but the intent and visual connection to these images and tradition should be maintained. The goal is to capture the feel and scale of the various styles. Much like attractive, established urban neighborhoods, the variety of architecture adds to the character and provides that individual look which is the goal of this development.

A. Craftsman Architecture

Craftsman architecture is common throughout Southern California in many forms. The Craftsman style in Southern California spun out of bungalows that were the "tract home" of the time. These designs were available by mail order and each was customized by the individual. Craftsman architecture was refined by architects, such as the Green Brothers and Maybeck, into a unique style that promotes hand crafted quality; thus the name "Craftsman". This type of architecture can be found in the classic tree-lined neighborhoods of Pasadena, Orange, and Santa Ana.

The character is established with the use of wood beams, deep overhangs with exposed plumb-cut rafters, and porches and verandas supported by large pillars. The predominant look is horizontal with stucco the primary material with wood siding and shingles also common. Brick and stone are used as accents on columns, pillars and wainscots. Roof materials are asphalt or concrete shingles. Colors are earthtones, both light and dark, with low contrasts between colors. A conceptual rendering of the Craftsman Architectural style is included on Exhibit 18.



NOTE: THE ARCHITECTURAL ELEVATION SHOWN HERE INDICATES THE VARIETY OF ARCHITECTURAL COMPONENTS ASSOCIATED WITH EACH STYLE. INDIVIDUAL HOMES MAY CONTAIN A PORTION RATHER THAN ALL COMPONENTS OF THE ASSOCIATED ARCHITECTURAL STYLE.

Exhibit 18

CRAFTSMAN ARCHITECTURE



California
LANDINGS

Kaufman & Broad ▲

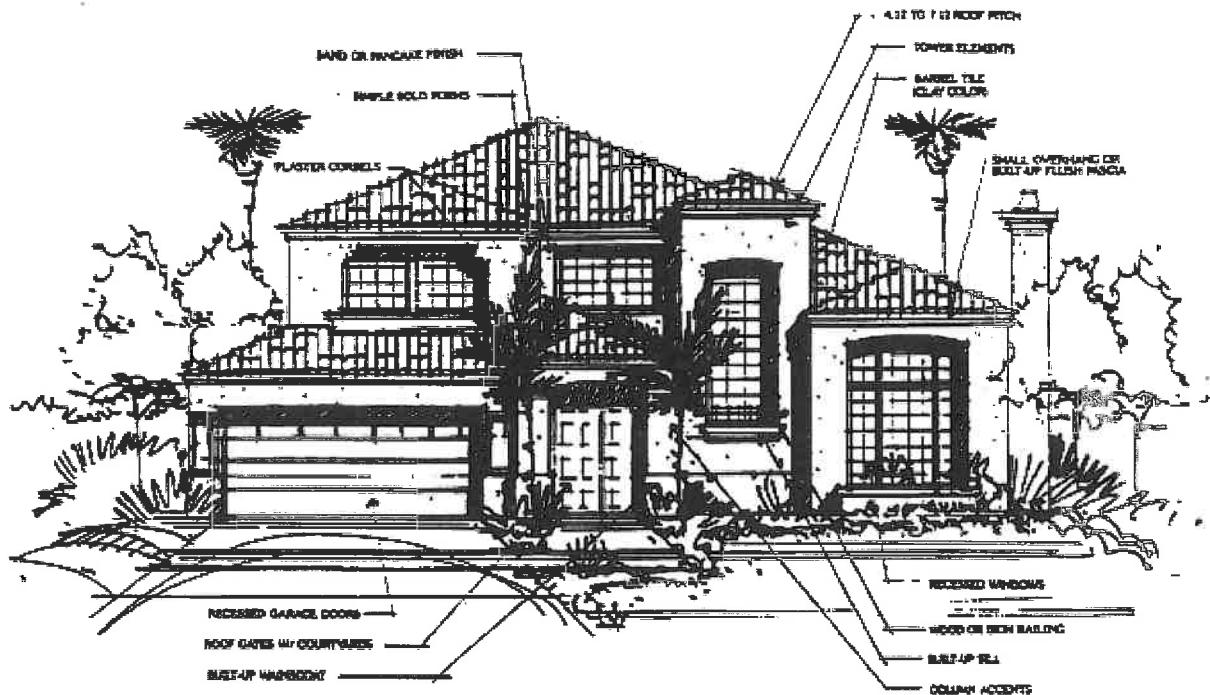
B. California Mediterranean

The architecture that dominates the coastal hillsides of the Mediterranean has been transplanted in Southern California. Similar in climate and topography, this migration was natural. The southern California vernacular, having its roots in the Mediterranean, is characterized with barrel tile roofs, recessed openings and sense of mass and solidity. The architecture of the Mediterranean, built in decades rather than years, has a timeless feel that, with simple forms and uncluttered detailing, the Southern California interpretation can result in a time honored look that responds to the climate, culture and marketplace of Southern California.

The Mediterranean style offers a variety of roof pitches, but will always have clay colored tile with a flush or small overhang. Hipped, gable and flat roofs with varying plate heights providing that image of being built over time are common place. The exterior materials are plaster (stucco) with predominately lighter colors and bright accent on rails, shutters and awnings. Exhibit 19 reflects the California Mediterranean architectural style presented above.

C. Prairie

The roots of Prairie architecture began in the late 1800's with the "Oak Park" and River Forest houses of Frank Lloyd Wright. The Prairie school of architecture is characterized with horizontal expression and delicate proportions. The roof often floats with deep overhangs. Stately, strong and weighty proportions provide a massive "earthy" feel. Windows are grouped in horizontal bands with vertical proportions. Oftentimes, 3/4 proportions are used to set up the high horizontal lines. Stucco or wood siding with horizontal emphasis are predominant with brick, stone or concrete block detailing. Roofs will be tile. Colors are earth-tone with both light and dark shades. The Prairie style is conceptually rendered on Exhibit 20.



NOTE: THE ARCHITECTURAL ELEVATION SHOWN HERE INDICATES THE VARIETY OF ARCHITECTURAL COMPONENTS ASSOCIATED WITH EACH STYLE. INDIVIDUAL HOMES MAY CONTAIN A PORTION RATHER THAN ALL COMPONENTS OF THE ASSOCIATED ARCHITECTURAL STYLE.

CALIFORNIA MEDITERRANEAN ARCHITECTURE

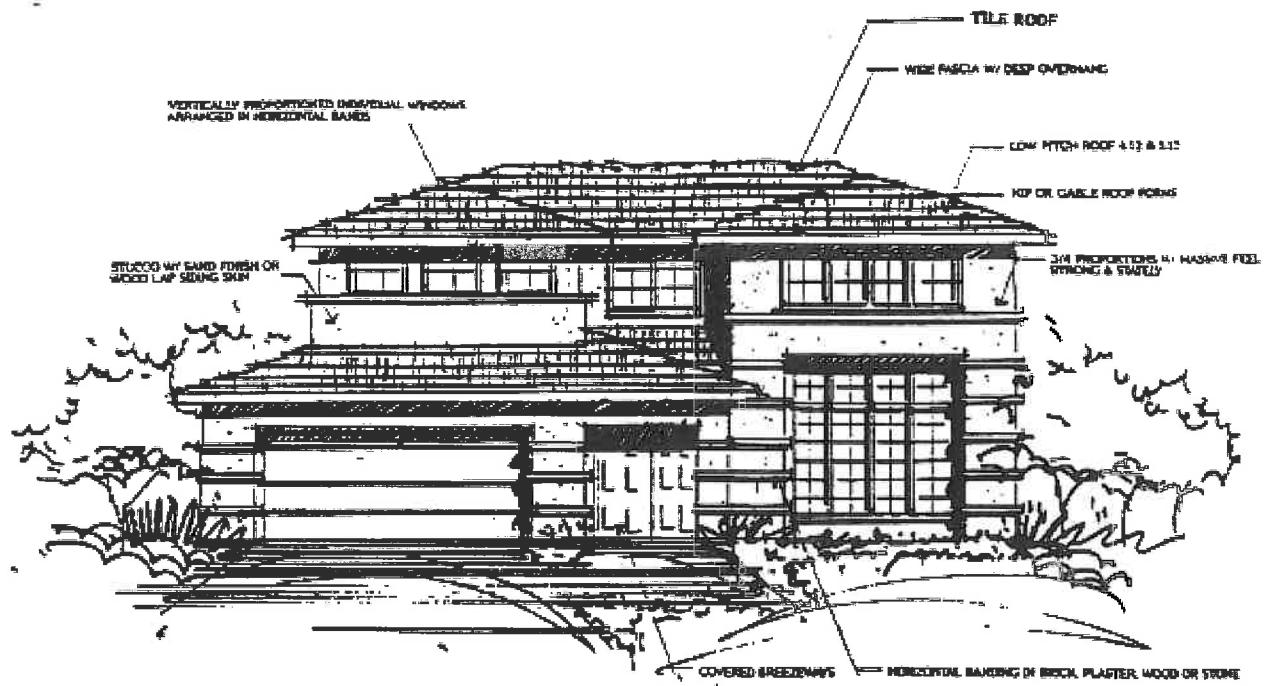


ELVA

California
LANDINGS

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Exhibit 18



NOTE: THE ARCHITECTURAL ELEVATION SHOWN HERE INDICATES THE VARIETY OF ARCHITECTURAL COMPONENTS ASSOCIATED WITH EACH STYLE. INDIVIDUAL HOMES MAY CONTAIN A PORTION RATHER THAN ALL COMPONENTS OF THE ASSOCIATED ARCHITECTURAL STYLE.

Exhibit 20

PRAIRIE ARCHITECTURE



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D. Early California Architecture

Early California architecture can be defined by many terms including Mission, Spanish Colonial and Monterey. The historical roots of all these styles come from several countries of the Mediterranean such as Spain, North Africa and Italy. The charm of these styles lies in the directness, adaptability and contrast of materials and textures. A variety of forms and massing, in an almost accidental arrangement, give a sense of being built over time. The charm of this style is represented in many Southern California locales. Santa Barbara, which was designed and built primarily in the 1920's and 1930's, and demonstrates many examples of early California architecture. The roof is barrel tile clay in color with plaster walls and neutral color in whites and off whites with pastel tints. The wood is natural weathered stain. Exhibit 21 conceptually renders the Early California Architectural style.

E. California Ranch

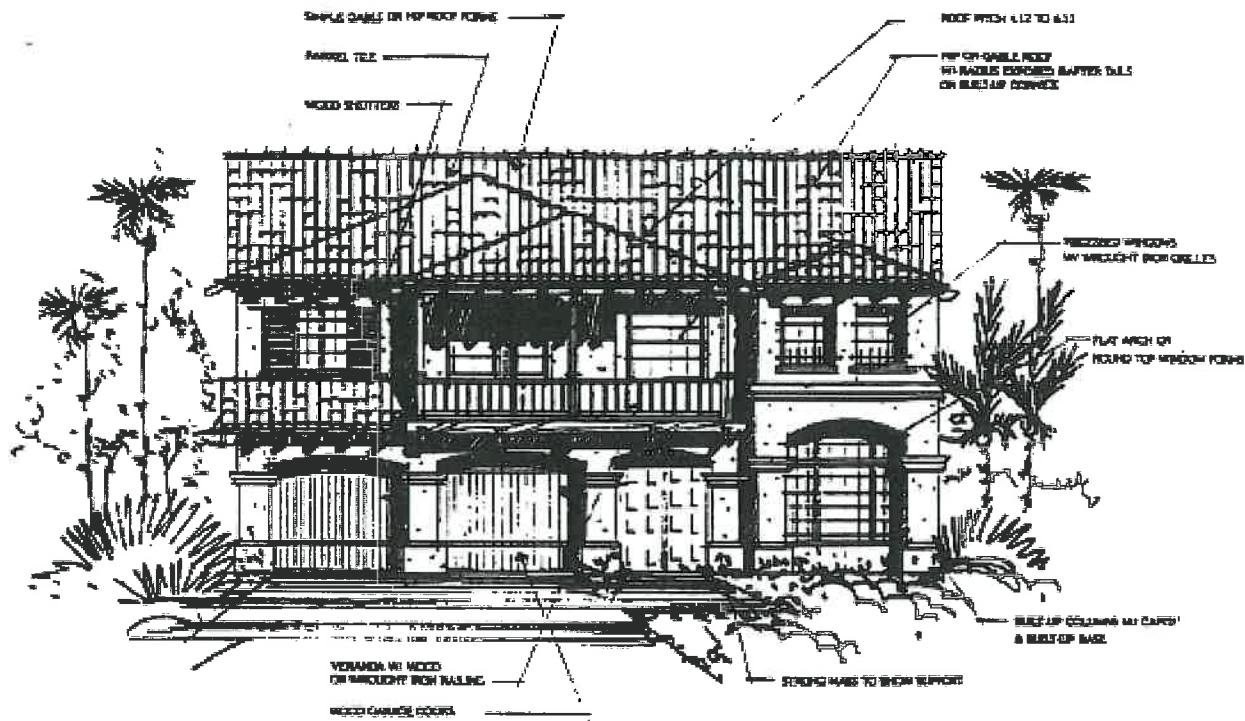
California Ranch style is derived from the early ranch architecture of the 1940's with bungalow and Mediterranean influences. California Ranch architecture became a distinct style with extensive use in the 1970's. It was characterized by a horizontal emphasis with hip and gable roofs varying in pitch from 4:12 to 5:12 and moderate overhangs. Indoor/Outdoor relationships are a form determinant to California Ranch architecture as large areas of glass, corner windows, greenhouse windows and continuous materials link indoor and outdoor spaces. Exposed beam ends and deep fascias with built-up wood post or masonry columns are typical.

Stucco and wood siding are the predominant wall materials with masonry accents in the form of columns or wainscots creating private gardens, patios or pot shelves. Roofs are asphalt shales or concrete tile. Colors are earth tone, in the beige family with brighter use of accent colors. The California Ranch style is conceptually rendered on Exhibit 22.

F. Spanish Monterey

Spanish Monterey architecture was developed in the 20's and 30's in the Monterey area of California. Spanish Monterey is the blending of two architectural traditions. Whalers from New England brought carpenters with them to the West Coast who were experts in the colonial forms and detailing. Spanish Monterey architecture can have a Spanish emphasis or a more New England dominance by using the Spanish methods common to the area.

The building forms are simple in one or two stories. Roofs feature gables or hips with broad overhangs, often with exposed rafter tails. When Spanish tradition is emphasized, the roof material should be barrel tile. Shutters, balconies, verandas, and porches are integral into the Spanish Monterey style. This style is rendered on Exhibit 23.



NOTE: THE ARCHITECTURAL ELEVATION SHOWN HERE INDICATES THE VARIETY OF ARCHITECTURAL COMPONENTS ASSOCIATED WITH EACH STYLE. INDIVIDUAL HOMES MAY CONTAIN A PORTION RATHER THAN ALL COMPONENTS OF THE ASSOCIATED ARCHITECTURAL STYLE.

Exhibit 21

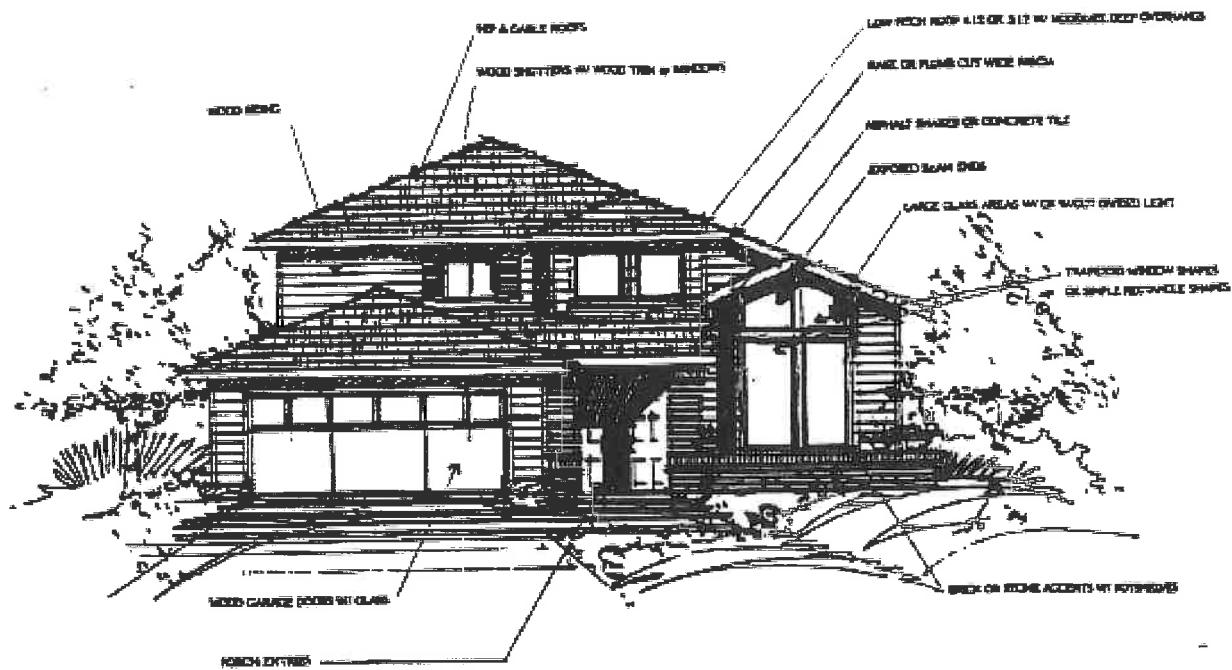
EARLY CALIFORNIA ARCHITECTURE



EARLY
CALIFORNIA
ARCHITECTURE

California
LANDINGS

Kaufman & Broad ▲



NOTE: THE ARCHITECTURAL ELEVATION SHOWN HERE INDICATES THE VARIETY OF ARCHITECTURAL COMPONENTS ASSOCIATED WITH EACH STYLE. INDIVIDUAL HOMES MAY CONTAIN A PORTION RATHER THAN ALL COMPONENTS OF THE ASSOCIATED ARCHITECTURAL STYLE.

Exhibit 22

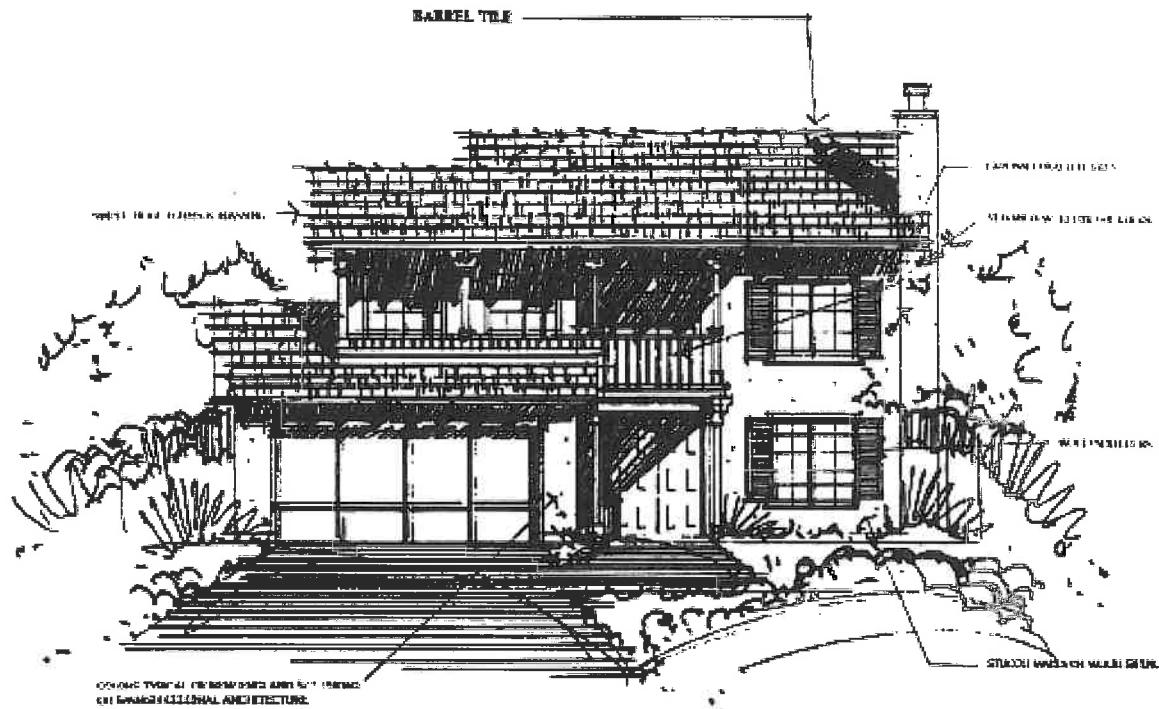
CALIFORNIA RANCH ARCHITECTURE



California

California
LANDINGS

Kaufman & Broad ▲



NOTE: THE ARCHITECTURAL ELEVATION SHOWN HERE INDICATES THE VARIETY OF ARCHITECTURAL COMPONENTS ASSOCIATED WITH EACH STYLE. INDIVIDUAL HOMES MAY CONTAIN A PORTION RATHER THAN ALL COMPONENTS OF THE ASSOCIATED ARCHITECTURAL STYLE.

Exhibit 23

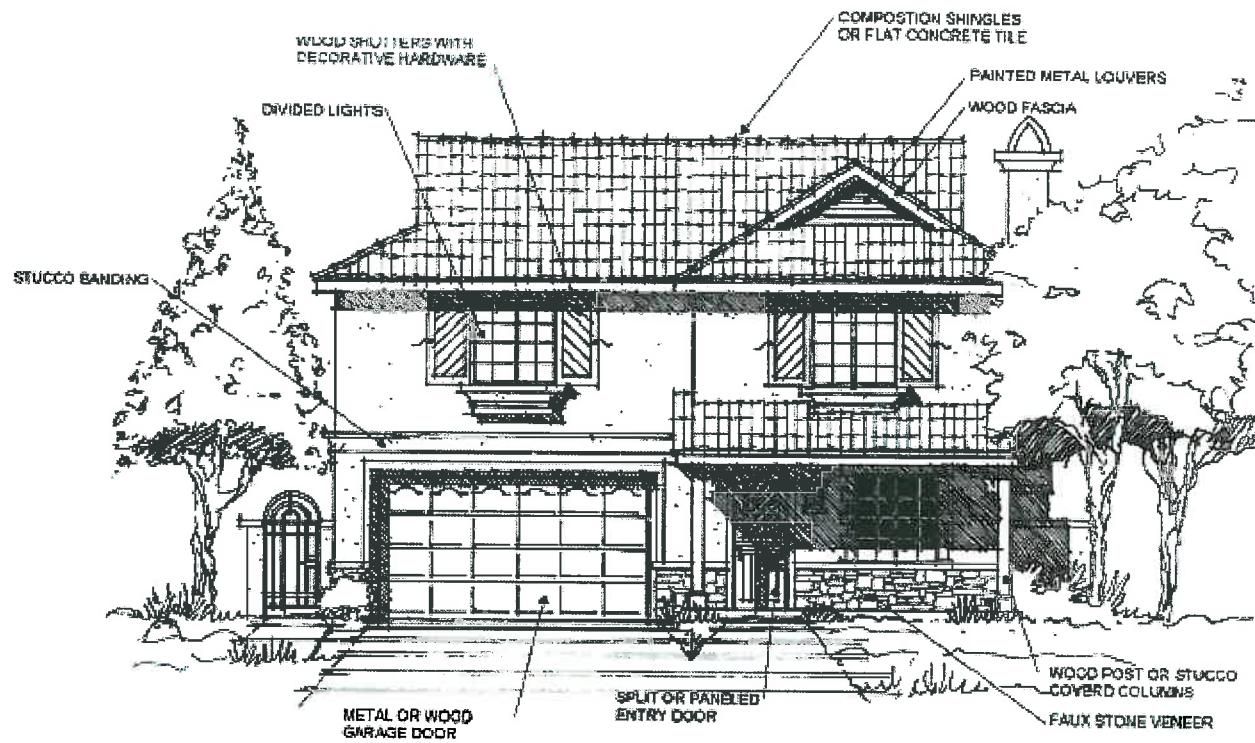
SPANISH MONTEREY



KAUFMAN & BROAD

California
LANDINGS

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NOTE: THE ARCHITECTURAL ELEVATION SHOWN HERE INDICATES THE VARIETY OF ARCHITECTURAL COMPONENTS ASSOCIATED WITH EACH STYLE. INDIVIDUAL HOMES MAY CONTAIN A PORTION RATHER THAN ALL COMPONENTS OF THE ASSOCIATED ARCHITECTURAL STYLE.

Exhibit 24

COTTAGE

California
LANDINGS

G. Cottage:

Originally from England in the Middle Ages, homes that would house agricultural workers called cotters, a Cottage is typically a small house. It may carry the connotation of being an old or old-fashioned building. In modern usage, a Cottage is usually a modest, often cozy dwelling, typically in a rural or semi-rural location.

Styles that became some of the defining vocabulary were Tudor and Norman Domestic. Holiday Cottages in various countries such as Norway, Sweden, Germany and Russia, immigrated to America and were built in the 1920s and later.

Roof shapes are either gables or hips. Roof pitches are usually more steep than other styles (6:12 or greater) and can be very exaggerated to where they are emulating architecture of children's stories. Sloping gables on the front elevation help emulate this exaggerated look. Roofing material would either be composition shingles or flat concrete tile. Wood fascias can be tight or moderate. Tudor wood trim patterns can be applied to smooth stucco covered walls. Decorative stone or brick patterns, along with painted metal gable louvers are common applications. Recessed entry ways can be arched or straight, along with prominent picture windows. Entry Doors can be either horizontal and vertical mullion patterns or diamond patterns. Colors are typically white or light earth tones. Trim colors should contrast by complement the base color of the home. Shutters or faux stone veneer can be added for color and texture.

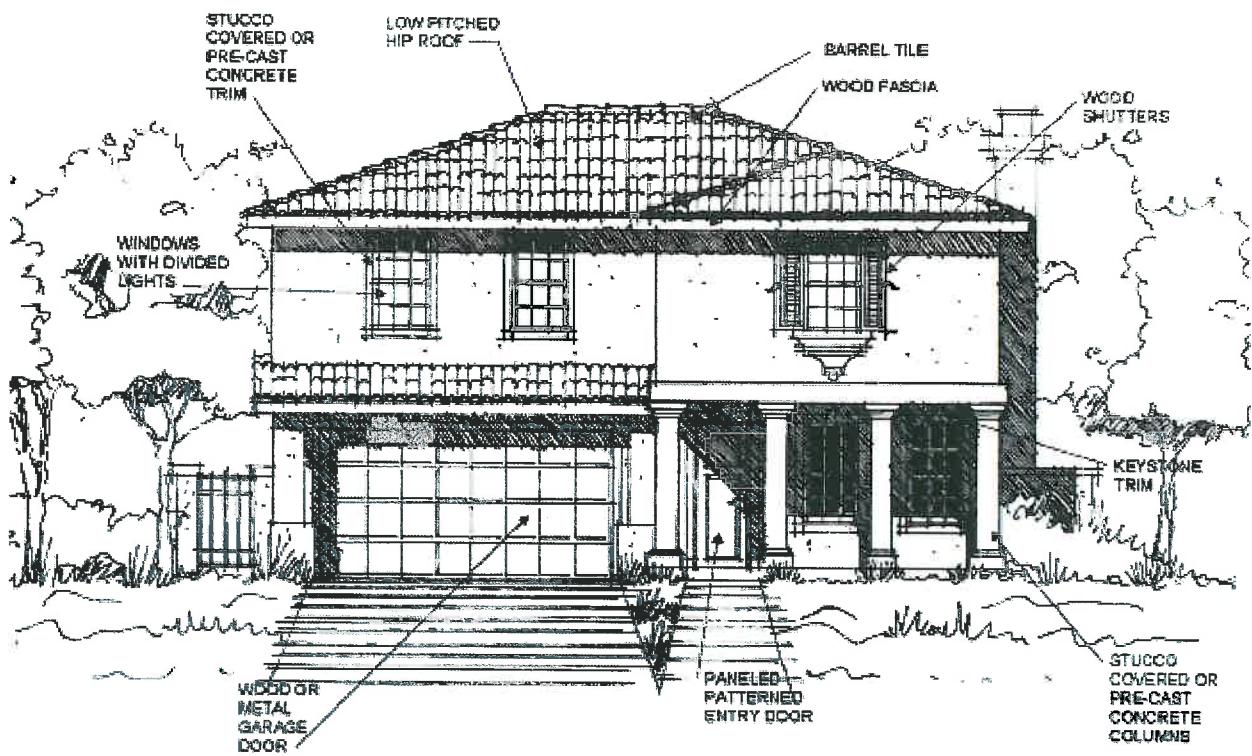
This style is rendered in Exhibit 24.

H. Italian Revival

A romantic style inspired by the Italian Renaissance that influence writers, artists, and Architects in the late 1800s to the 1930s. The style developed from the country Italian architecture of areas like Tuscany and became more refined in its detailing to reflect the more refined building materials and the skills of higher level artisans.

Italian Revival's characteristics show hipped roofs at lower pitches (5:12 or lower) with clay or concrete barrel tiles with a tight to moderate overhang. Facias can be standard wood, built-up stucco, or exposed corbels or rafter tails. Exterior walls to be of a stucco finish and could have stone or brick veneer bases. Pre-cast (either concrete or stucco covered wood or foam) window and door trims with keystone at prominent elements or entry points help define the style. Windows can be of a pattern rectilinear or square pattern with mullions showing smaller window pane patterns or can incorporate Roman arched windows at prominent locations. Wood shutters, either paneled or louvered with decorative hardware are very typical. Along with paneled entry doors and garage doors. Main wall colors can be whites, beiges, or greys with contrasting trim colors.

This style is rendered in Exhibit 25



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Exhibit 25

ITALIAN REVIVAL

California
LANDINGS

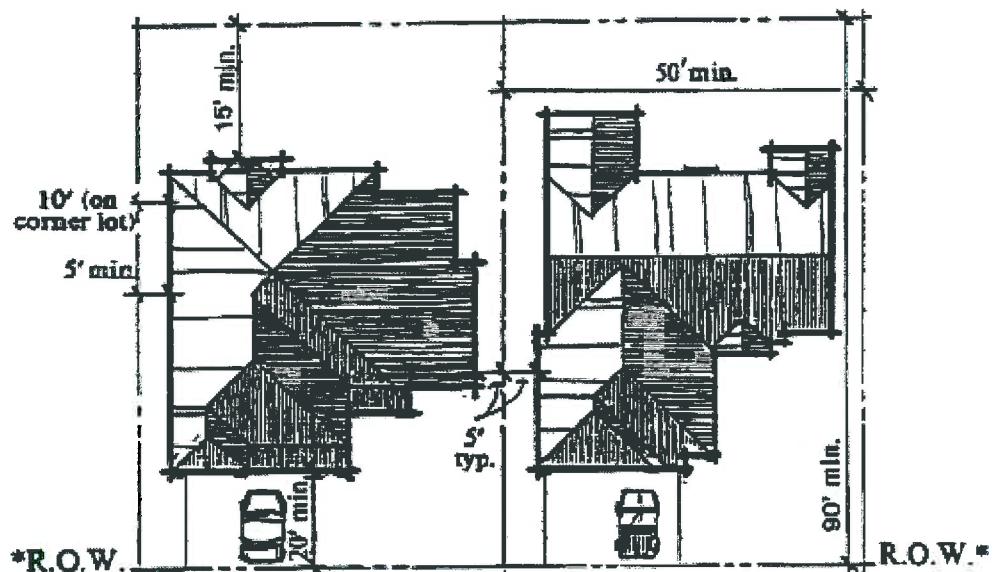
4.3.3 Design Guidelines

RESIDENTIAL

- For each residential floor plan at least two complementary elevations will be provided to insure interest and create a varied neighborhood street scene.
- Where two residential units of the same floor plan are located on adjacent single family lots, one will be a reverse plan or different in elevation from the other of the same plan.
- The total building and its related architectural elements should be visually unified to provide order and coherence.
- The design of improvements should complement the natural order through materials, textures and colors.
- Materials, colors and general style should be integrated through each residential development site to achieve continuity in design.
- The height and bulk of buildings should be approximate in scale to the size, shape and topography of the site and harmonious with the setting, verified through grading concept plans and site plan review.
- Building faces (particularly front elevation planes) should be varied in placement and size to provide interest and focuses, and to create human scale.
- Further architectural articulation of building faces and roof planes can be accomplished through the introduction of architectural supplements such as architectural projections, dormers, roof ridge jogs, roof overhangs, building face trims, recessed doorways, bay windows or entry courts.
- In two story structures, scale and vertical transition can be created in the front of the structure by "stepping back" the second story and providing a partial roof or trellis at the top of the first floor level.
- Accent windows having a different or articulated top (e.g., rounded, diamond and/or with contrasting moldings) or with a finer texture (e.g., many small panes) should be used to create interest on building elevations.

- To provide interest and/or architectural accents on structures predominantly finished in stucco; moldings, cornices, insets or offsets can be used. Similarly, to create interest, portions of the exterior building surface can also be covered with a complementary building material, texture or color.
- Driveway materials that retain their original beauty and strength such as concrete, brick, tile or conglomerate are encouraged.
- Roof appurtenances, such as vents or flashing, should be positioned away from the street side of the house or finished to match the roof color in order to minimize the 'visual impact.'
- Any bare metallic surface (vents, pipes, gutters, flashing, etc.) should be painted or covered from view in a manner harmonious with the general exterior architectural treatment of the building.
- Architectural screens, fences and accessory structures should be compatible in material, color and "textile" to the main building.
- The rear and side elevations of residential units facing to the local and collector streets should have enhanced architectural treatment. Enhanced treatment can consist of the following:
 1. Window trim.
 2. Facade plane variation of no less than six (6) inches.
 3. Incorporation of materials used on the front facade.

- Ground level building elevations that are completely screened from public view, such as rear yard service and trash areas, could have reduced window area and detailing.
- Provide adequate separation between parking and store fronts to allow for comfortable pedestrian spaces. Vary this distance to provide visual interest.
- Columns, projections and exterior detailing shall be used to architecturally identify and enhance building entries.
- All rooftop equipment shall be screened or otherwise kept from public view. Screening element shall be integrated into building design.
- Building exterior openings (doors and windows) should be used in a repetitive pattern to create rhythm but avoid monotony.
- Signing should be an integral part of commercial building design and architecture and should comply with the City's Sign Code as outlined in Chapter 3 of the Building Code.



TYPICAL PLOTTING

MINIMUM LOT SIZE	6,200 SQ.FT.
MAXIMUM COVERAGE	50%
MINIMUM PARKING SPACES	2 VEHICLE GARAGE PER UNIT

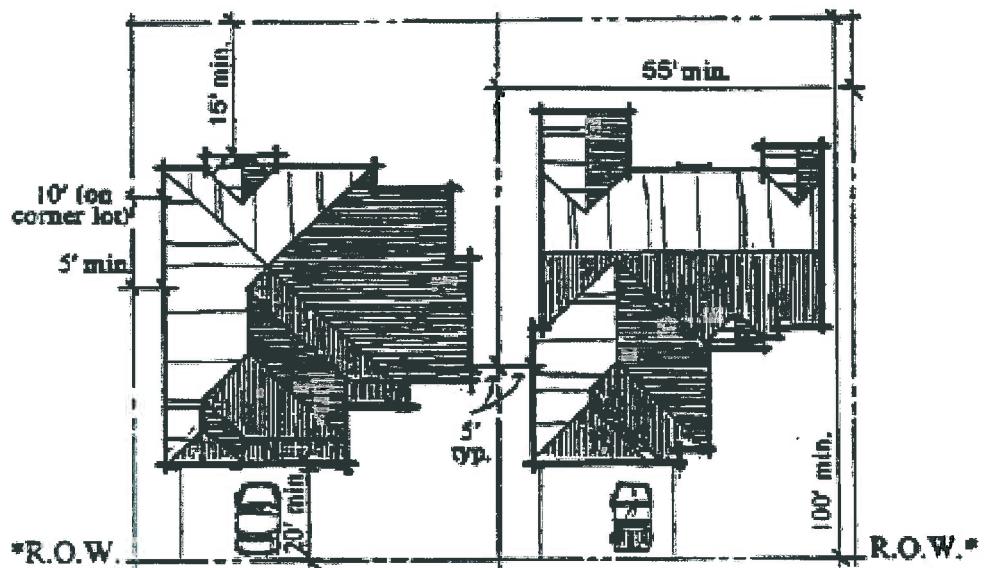
Exhibit 26

SINGLE FAMILY LOTS
6,200 SQ. FT.



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LANDINGS



TYPICAL PLOTTING

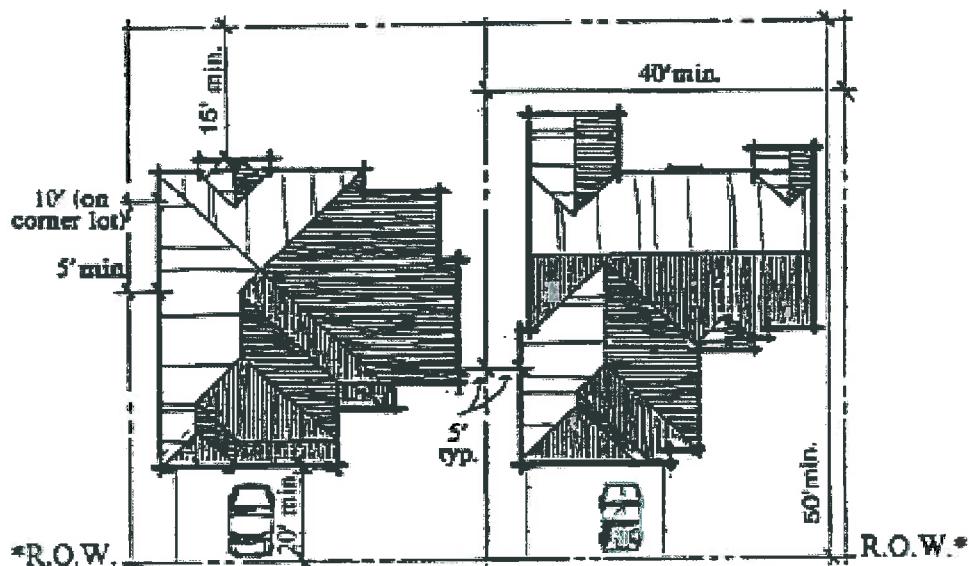
MINIMUM LOT SIZE	7,200 SQ.FT.
MAXIMUM COVERAGE	50%
MINIMUM PARKING SPACES	2 VEHICLE GARAGE PER UNIT

SINGLE FAMILY LOTS
7,200 SQ. FT.

Exhibit 27



California
LANDINGS



TYPICAL PLOTTING

MINIMUM LOT SIZE	5,000 SQ.FT.
MAXIMUM COVERAGE	60%
MINIMUM PARKING SPACES	2 VEHICLE GARAGE PER UNIT

Exhibit 28

SINGLE FAMILY LOTS 5,000 SQ. FT.



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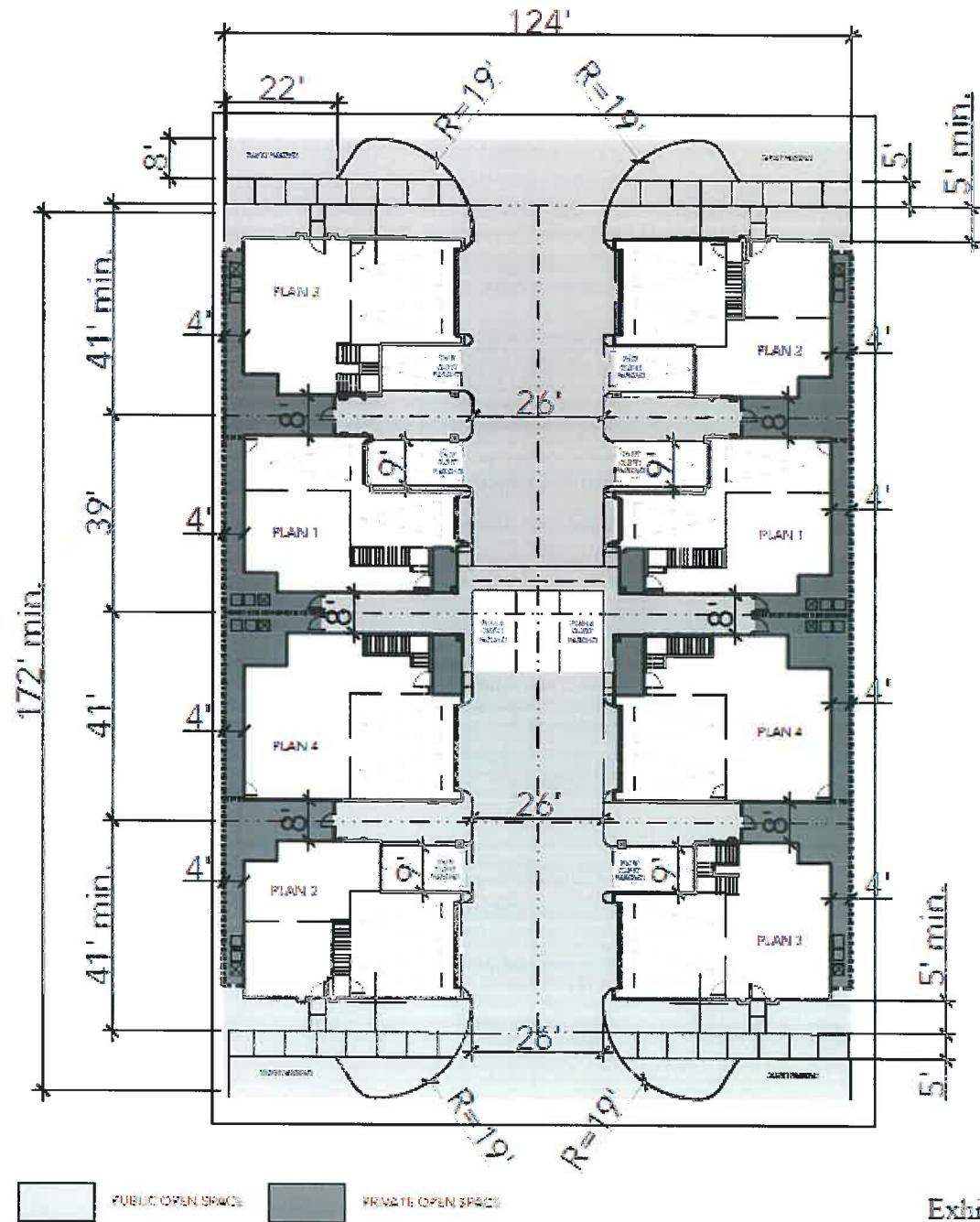


Exhibit 29

TYPICAL DETACHED CLUSTER - 8 PACK

California LANDINGS

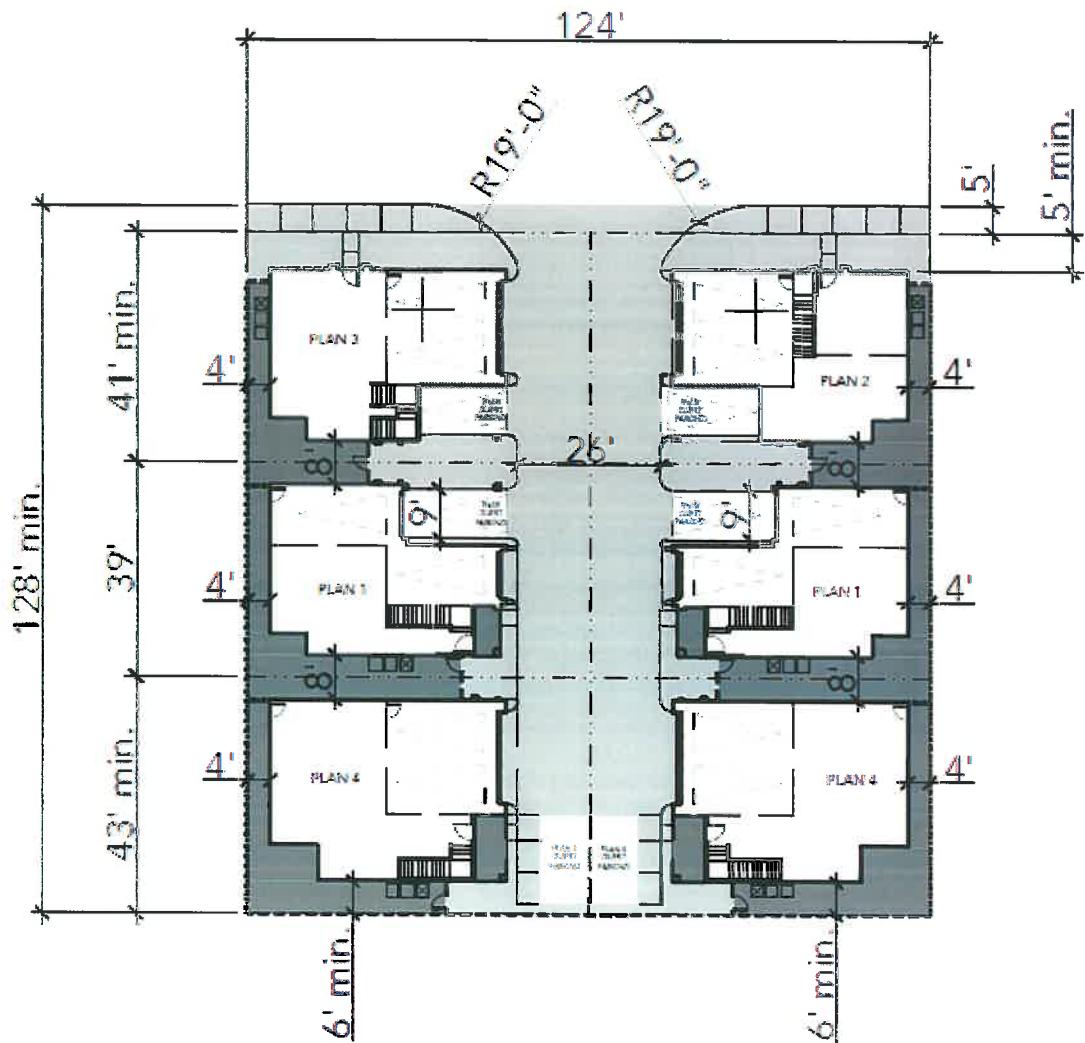


Exhibit 30

**TYPICAL
DETACHED
CLUSTER - 6 PACK**

California
LANDINGS

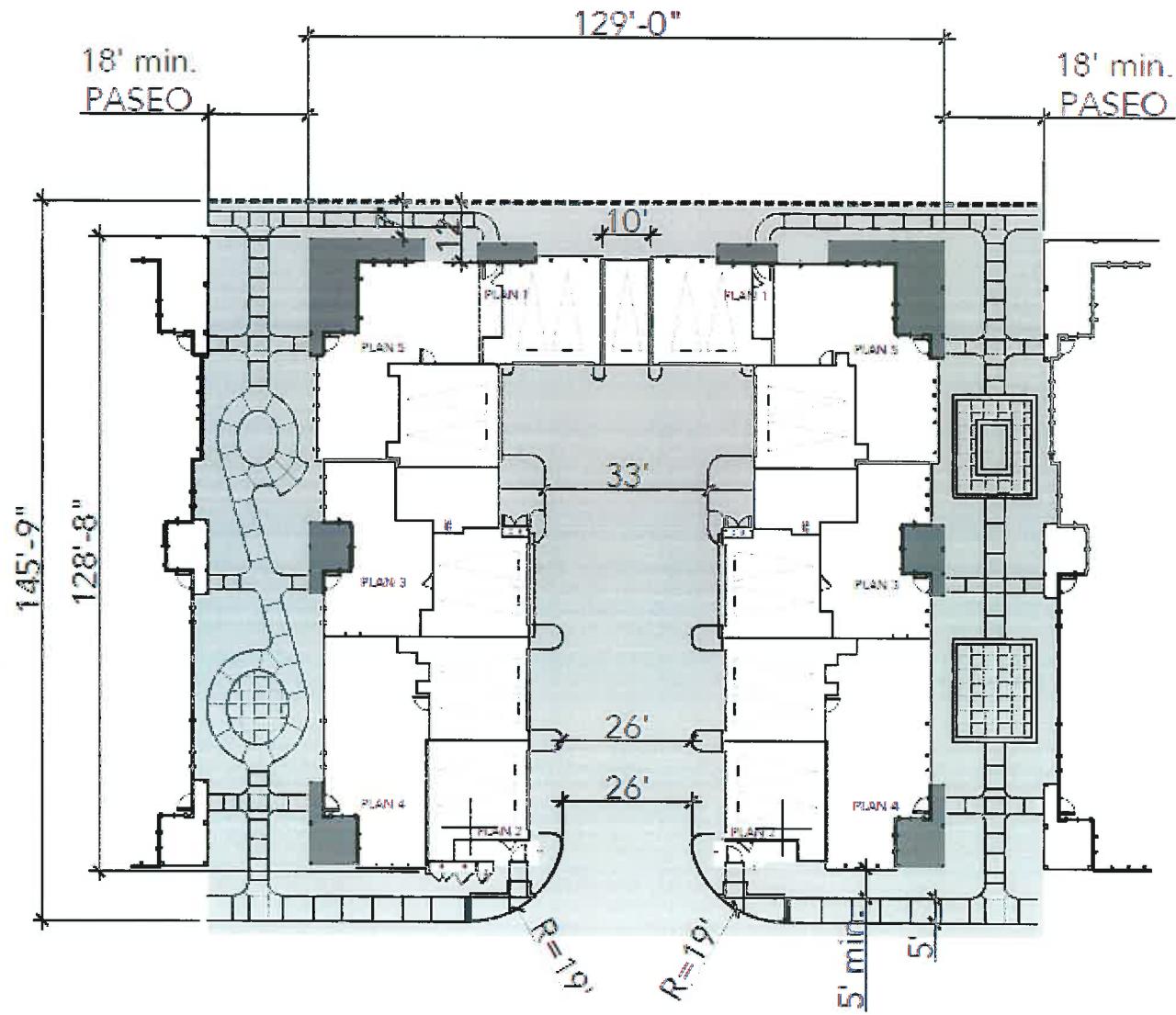


Exhibit 31

**TYPICAL
ATTACHED
CLUSTER**

California
LANDINGS

4.4 LANDSCAPE PLAN

This section contains the design concepts, guidelines and special treatment plans for the implementation of landscaping within the California Landings Specific Plan area. These concepts, guidelines and special treatments are intended to:

- Enhance the visual quality and aesthetic characteristics of the community;
- Promote a consistent character of development and community identity;
- Create a pleasing visual image for community residents and visitors;
- Enhance the open space areas of the site.
- Integrate community uses through the design of a pedestrian/open space trail network.

The major elements of the California Landings Landscape Program as discussed in the following sections include:

- Community Landscape Concept
- Community Monumentation;
- Recreation and Amenities.

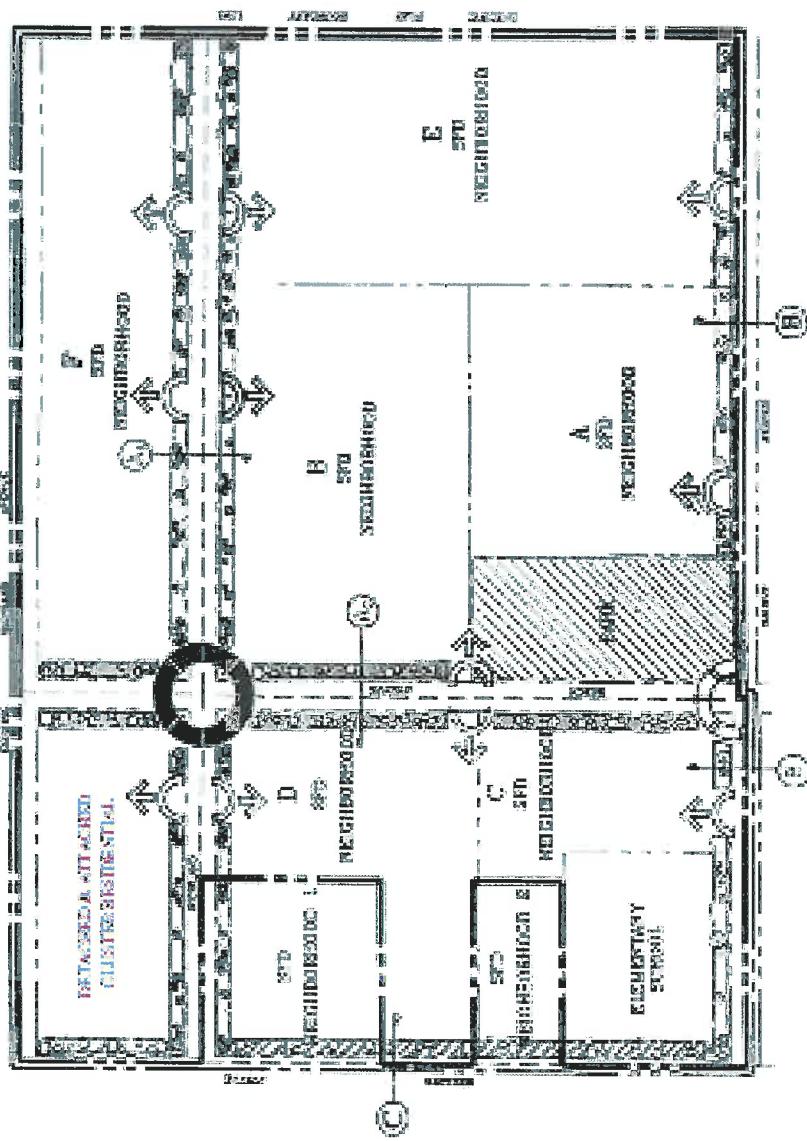
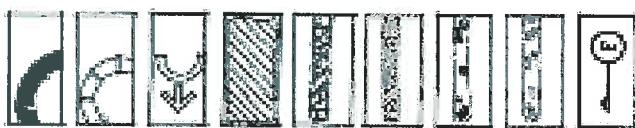
Community Landscape Concept

The community landscape concept (see Exhibit 32) is intended to provide identity and character to the various residential areas of the Specific Plan. In this regard, the landscape concept enhances the natural features of the area through accentuation of distinct land uses, integration of architectural form with the landscape, and repetition of a common theme. Landscape features have been included which are designed to enhance the project's interface with the surrounding environment and the adjacent community. The landscape concept suggests methods of treatments which might be considered; alternative landscape concepts and designs which accomplish the stated objectives and intentions are permitted. More detailed landscape design information, including a master list of tree and plant materials is provided in the Appendix, see Section 73.

MASTER LANDSCAPE PLAN

MEMOIR OF LITERATURE

- 1. **THEATRE STUDIES**
- 2. **SCENIC ARTS**
- 3. **PERFORMANCE STUDIES**
- 4. **THEATRE DESIGN**
- 5. **THEATRE HISTORY**
- 6. **THEATRE PRACTICE**
- 7. **SCENIC DESIGN**
- 8. **THEATRE STUDIES**
- 9. **THEATRE LINE**



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Streetscapes: Streetscapes have been designed to promote community character and complement the form and function of the various roadways. Streetscape treatment will be established along the arterial highways as well as local streets within the community. The master streetscape plan will consist of trees, shrubs and groundcovers which are selected to establish a distinct character or theme for each neighborhood. Consistency will be achieved through the use of plant materials selected from a master list of plants established for each streetscape or neighborhood.

Primary Highway: Highland and Beech Avenues (See Exhibit 33)
Parkway - Informal grove planting of deciduous and evergreen trees punctuated by small groupings of flowering accent trees.
Meandering walk with turf and accent shrubs and vines.

Median - Regular, on center planting of scale spreading evergreen trees with turf or groundcover below.

Divided Collector: Walnut Avenue (See Exhibit 34)
Parkway - Informal groves of spreading evergreen trees interspersed with groves of flowering accent trees with shrubs, groundcover and turf below.

Collector Street: Hemlock Avenue (See Exhibit 35)
Parkway - Formal, on-center planting of vertical evergreen trees with walk against curb. Accent planting of shrubs and vine pockets will be provided adjacent to walls.

Community Monumentation: The landscape concept for community monumentation consists of landscape treatment and monumentation to identify entry into the California Landings project. Entryway treatments and monuments will be designed to promote a traditional image. Product entry will focus on the individual character associated with the various product types.

STREETSCAPE SECTIONS

SECTION B (86' R.O.W.)
DIVIDED COLLECTOR
• WALNUT AVENUE

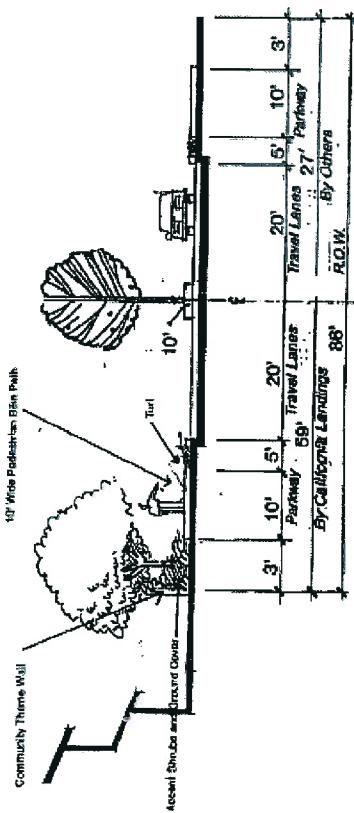


Exhibit 34

California
LANDINGS

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STREETSCAPE SECTIONS

SECTION C (68' R.O.W.)
COLLECTOR STREET
• HEMLOCK AVENUE

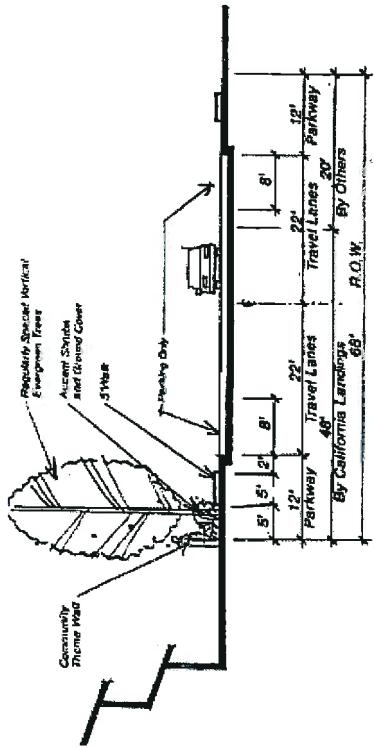


Exhibit 35

Kaufman
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Kaufman **A** Broad

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Primary Entry:

Stone clad entry wall to serve as community “gateway” with a large scale, Canopy accent tree as backdrop to reinforce arrival. Curving, stone-clad community identification wall intersects entry wall and has a back drop of a formal grove of flowering accent trees. Refer to Exhibit 36.

Secondary Entry (Residential):

Low, curving stone-clad wall on an elevated berm identifies this significant entry/intersection within the community. Formal grove of flowering accent trees serves as backdrop. Hardscape elements and plant material selections recall the theme of the primary entry. Refer to Exhibit 37.

Commercial Entry:

Commercial center identification on low curving stone sign wall provides the opportunity to reflect community identity through material and compatible landscape elements, such as a large specimen tree reflective of the secondary residential entry opposite this commercial entry. Refer to Exhibit 39.

Secondary Entry (Residential):

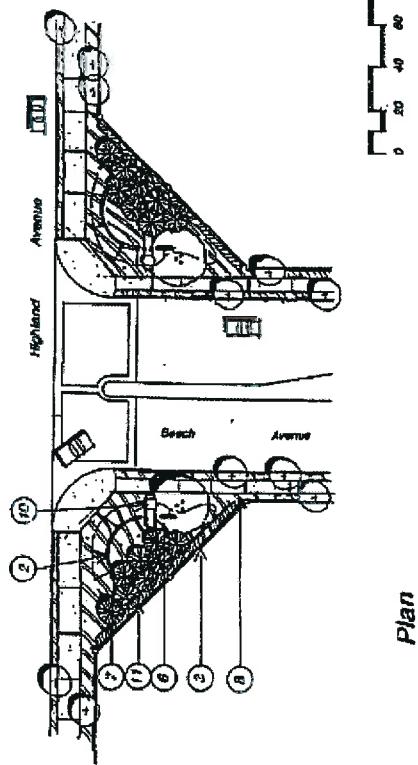
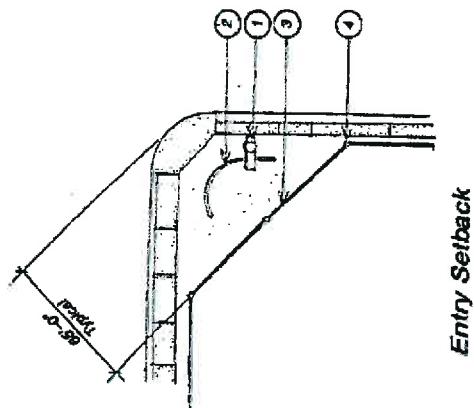
Low, curving stone-clad wall on an elevated berm identifies this significant entry/intersection within the community. Formal grove of flowering accent trees serves as backdrop. Hardscape elements and plant material selections recall the theme of the primary entry. Refer to Exhibit 37.

Cultured stone will be used in all entry locations where stone is specified for entry walls and wall pilasters.

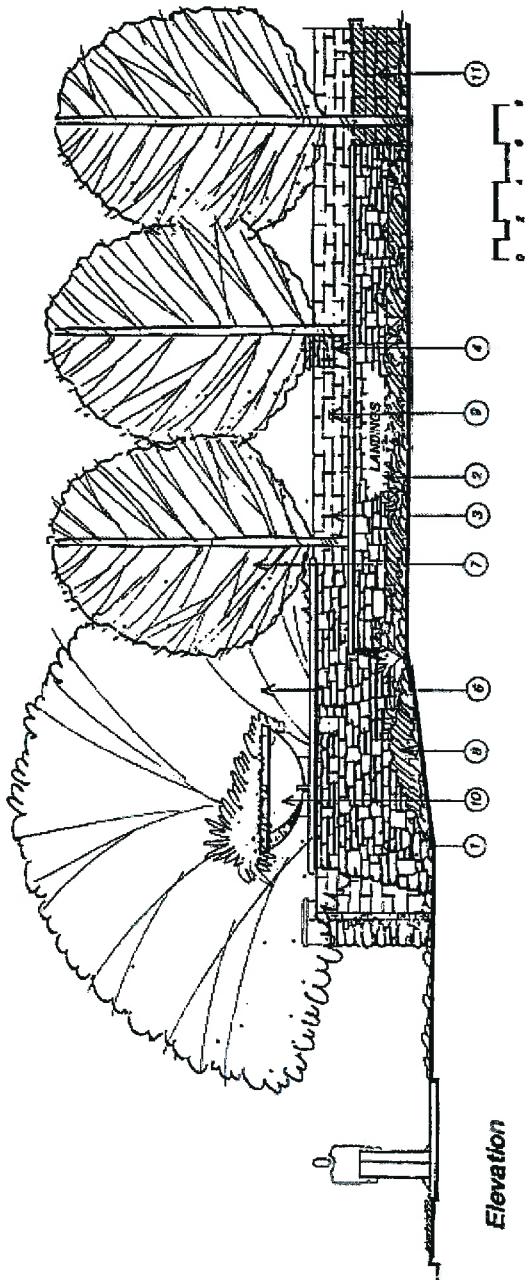
PRIMARY ENTRY PLAN AND ELEVATION

LEGEND:

1. STONE-CLAD ENTRY WALL
2. STONE-CLAD SIGN WALL
3. COMMUNITY THEME ENTRY WALL
4. STONE-CLAD PILASTER
(OCCURS ONLY AT ENTRIES)
5. NOT APPLICABLE
6. CANOPY TREE
7. ACCENT TREE
8. ACCENT SHRUB PLANTING
9. ACCENT VINE PLANTING
10. LOW PLANTER BOWL
11. LOW HEDGE



Plan



Elevation

NOTE: SAFE STOPPING AND CORNER
SIGHT DISTANCES AT INTERSECTIONS
WILL BE AS PER CALTRANS STANDARDS
UNLESS OTHERWISE APPROVED BY
FONTANA CITY ENGINEER.

Exhibit 36

(FOR ILLUSTRATIVE PURPOSES ONLY)

4-34

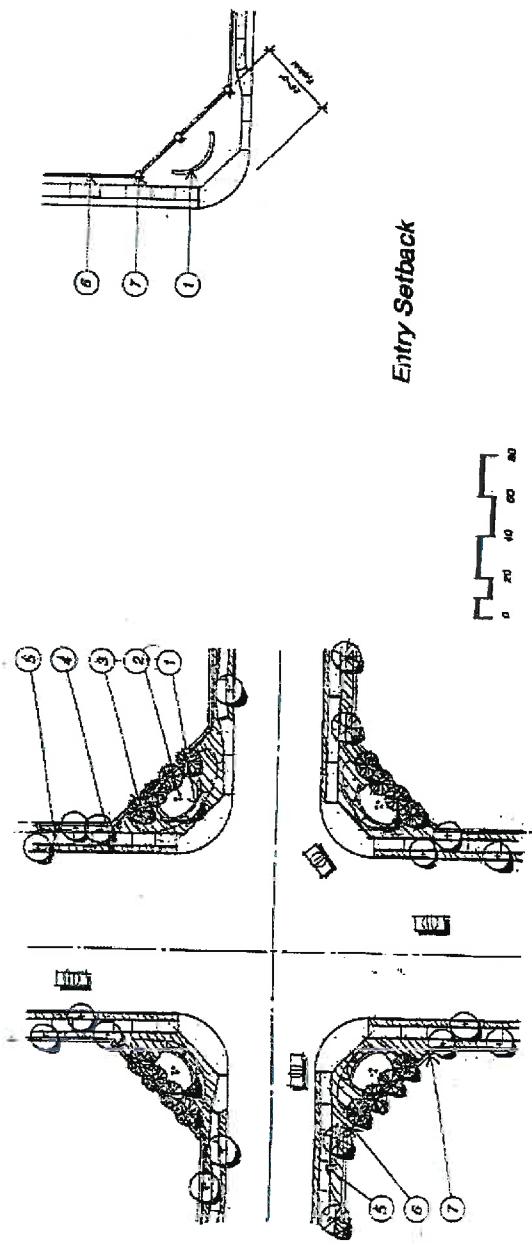
Kaufman Broad

California
LANDINGS

SECONDARY ENTRY PLAN AND ELEVATION

LEGEND:

1. LOW STONE-CLAD SIGN WALL
2. CANOPY TREE
3. ACCENT TREE
4. ACCENT SHRUB PLANTING
5. LINEAR WALK
6. COMMUNITY THEME WALL
7. STONE-CLAD PLASTER
(OCCURS ONLY AT ENTRSES)
8. ACCENT VINE PLANTING



NOTE: SAFE STOPPING AND CORNER
SIGHT DISTANCES AT INTERSECTIONS
WILL BE AS PER CALTRANS STANDARDS
UNLESS OTHERWISE APPROVED BY
FONTANA CITY ENGINEER.

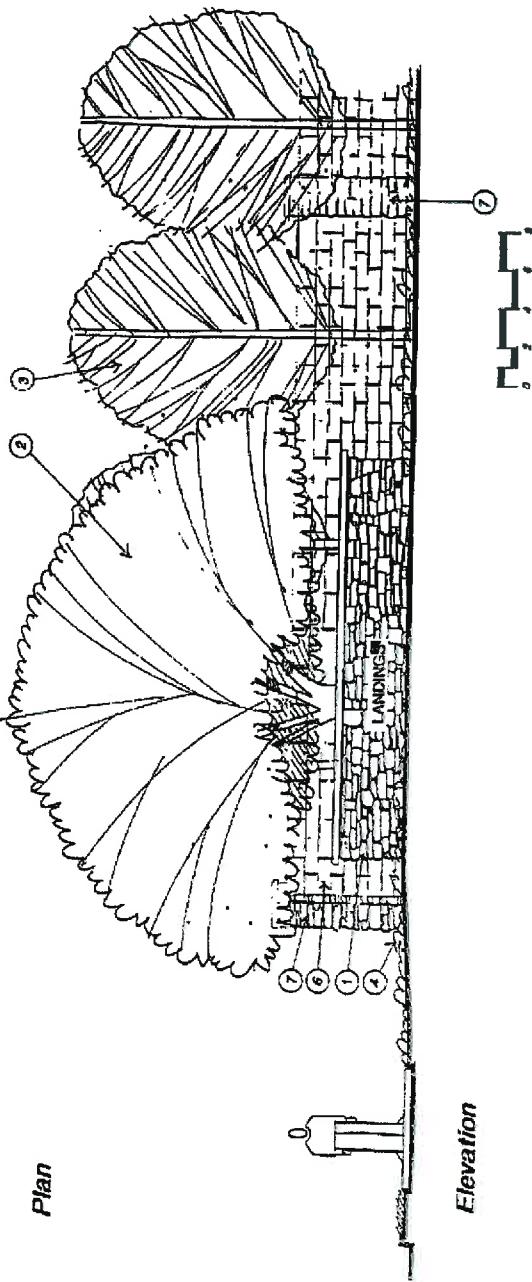


Exhibit 37

(FOR ILLUSTRATIVE PURPOSES ONLY)

4-35

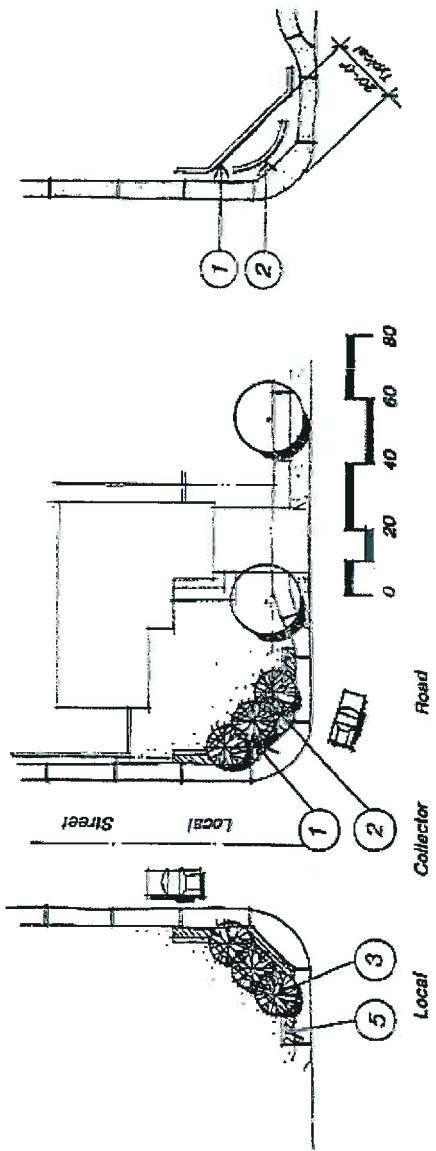
Kaufman  Broad

California
LANDINGS

NEIGHBORHOOD ENTRY PLAN AND ELEVATION

LEGEND.

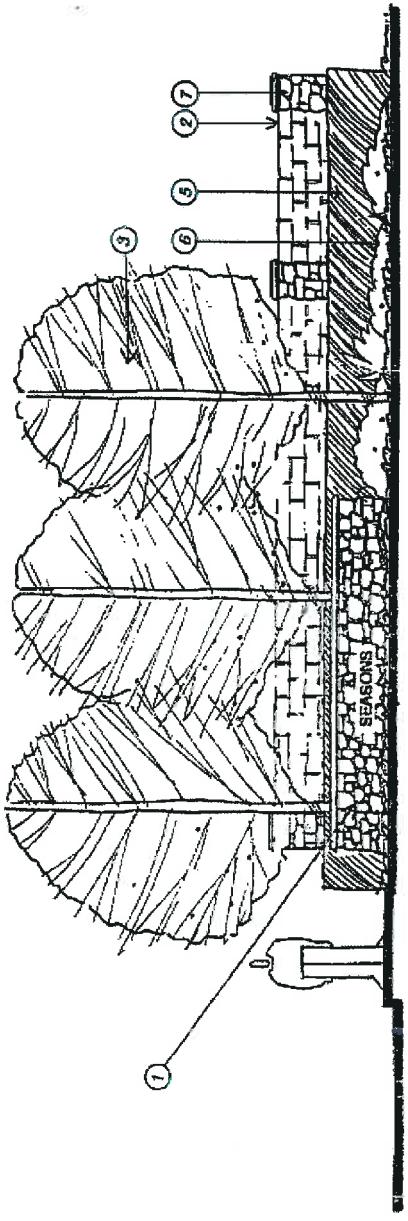
1. STONE-CLAD SIGN WALL
2. COMMUNITY THIMBLE WALL
3. ACCENT TREE
4. LOW CLIPPED HEDGE
5. ACCENT SHRUB AND VINE PLANTING
6. STONE-CLAD PILASTER (OCCURS ONLY AT ENTRIES)
7. FRONT YARD STREET TREE



Plan

Entry Setback

NOTE: SAFE STOPPING AND CORNER
SIGHT DISTANCES AT INTERSECTIONS
WILL BE AS PER CAITRANS STANDARDS
UNLESS OTHERWISE APPROVED BY
FONTANA CITY ENGINEER.



Elevation



Exhibit 38

(FOR ILLUSTRATIVE PURPOSES ONLY)

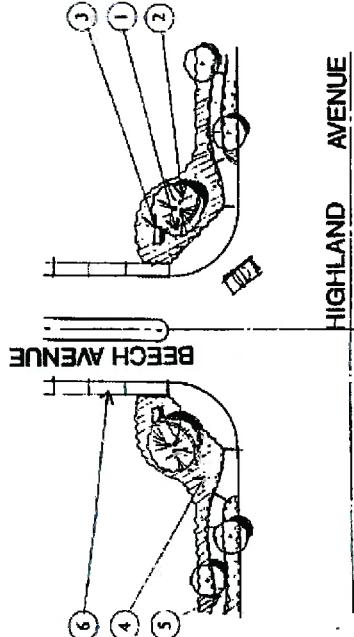
California
LANDINGS

Kaufman Broad

COMMERCIAL ENTRY MONUMENT

LEGEND:

1. LOW STONE-CLAD WALL
2. CANOPY TREE
3. STONE-CLAD PILASTER
4. ACCENT SHRUB PLANTING
5. MEANDERING WALK
6. LINEAR WALK



NOTE: SAFE STOPPING AND CORNER
SIGHT DISTANCES AT INTERSECTIONS
WILL BE AS PER CAUTRANS STANDARDS
UNLESS OTHERWISE APPROVED BY
FONTANA CITY ENGINEER.

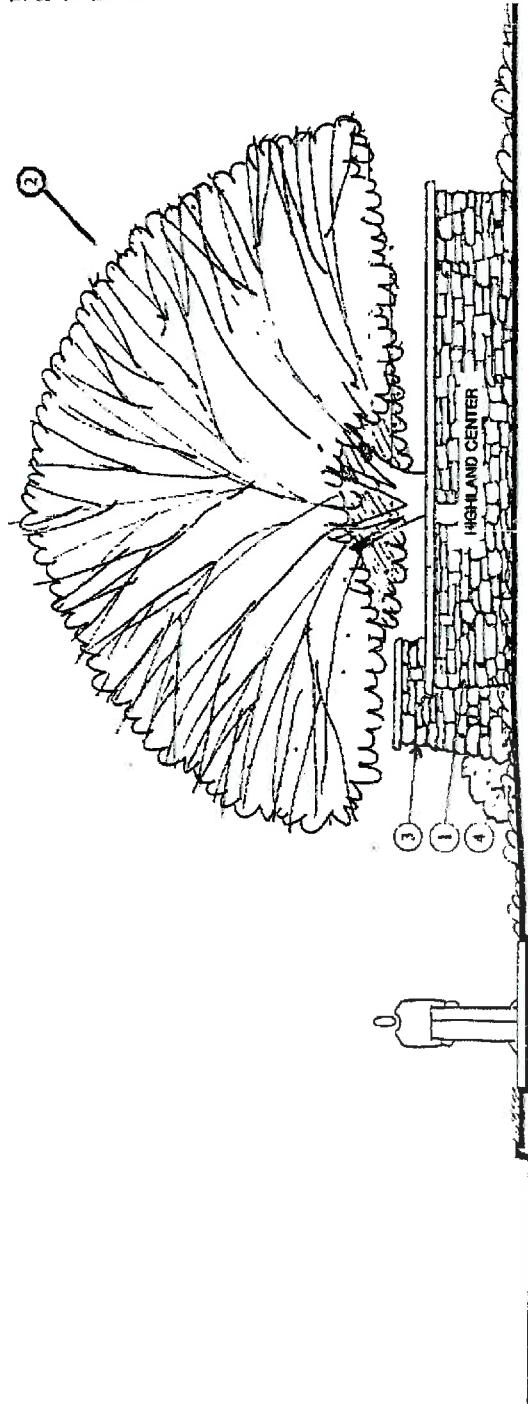


Exhibit 39

(FOR ILLUSTRATIVE PURPOSES ONLY)

4-37

Kaufman Δ Broad

Caltrans
LANDINGS

Park:

The landscape concept for the park centrally located on 10.0 acres consists of a special theme treatment. The park theme and commemorative elements will recall the historical use of the site as Gilfillan Field Airport and Weather Station. Refer to detailed plan in Section 3.5.

The following recommendations are suggested as general guidelines for community-wide implementation of the landscape concept.

- Plant material should be used to integrate the architectural form with the landscape. Extensive horizontal and vertical surfaces should be modulated or interrupted by foliage masses;
- Street tree plantings shall, in most instances, be planted in informal groupings;
- Street tree plantings along arterials and collectors may include areas of mounded turf or mounded shrub plantings;
- Streetscapes along primary highway, secondary roads and local collector roads should maintain a consistent landscape character with separate product identity statements;
- Landscape treatment and entry monumentation should serve to identify entry into each development area, establishing individual identification for various product types;
- Entries into California Landings should be accent points, announcing entry and introducing the theme and character of the community through use of vertical trees and accent shrub plantings;
- Planting material should be installed on manufactured slopes over 5 feet high to create a visual buffer and maintain a naturalistic relationship with natural open space areas;
- Landscaping details, including fencing, project walls and monuments, should enhance the theme of the community and serve to identify the individual development areas.

A plant palette has been established subject to final approval by City and is provided in the Appendix, see Section 7.3.

4.5 PEDESTRIAN AND BICYCLE TRAILS

Trails for California Landings will be provided as described in this Specific Plan. The guidelines will include trails for pedestrian and bicycle use. Exhibit 41 indicates the location of these trails and the various road sections indicate dimensions.

4.5.1 Trail Guidelines

1. On-site pedestrian circulation systems will be provided to meet the movement needs of on-site users. Such systems will provide all-weather surfaces, and aesthetically pleasing means of on-site foot travel. Pedestrian walkways will be an integral part of the overall architecture and site design concept.
2. Pedestrian and bicycle access will be conveniently provided to connect neighborhoods to schools, park and commercial uses. The number of pedestrian and bicycle crossings of streets will be limited and consolidated.
3. Where access is desirable, potential connections will be made between on-site and off-site perimeter circulation systems. Potential connections to off-project SFD neighborhoods 1 & 2 are indicated on the Bike and Trail Plan.
4. Pedestrian circulation system dimensions and surface materials shall be governed by minimum City standards. Where no standards exist a minimum 8 foot right-of-way will be encouraged, with four (4) feet of paved surface in locations such as access to the park from an adjacent residential area.

4.5.2 Bicycle Path Guidelines

1. Class I bike path (off-street, paved paths) will be located in the north side parkway of Walnut Avenue as a ten (10) foot wide combined bike path and walkway (see Exhibit 10, p. 3-13).
2. Class II or III bike lanes (on-street) will be located in the street (Beech, Highland and Hemlock Avenues) and will be a minimum of 4 feet wide.
3. Bicycle signage will be provided consistent with Caltrans and the City of

Fontana standards, and a six (6) inch wide solid stripe will be used to separate bike lanes from vehicular traffic

WALL & FENCE PLAN

LEGEND:

- COMMUNITY THEME WALL
 - (SPAT FACE BLOCK WITH PLASTERS
AT 10' O.C. 6' HIGH)
- PERIMETER WALL
 - (CLASSEMENT BLOCK WALL WITH NO
PLASTERS 6' HIGH)
- PARK WALL
 - (DECORATIVE MOLD)
- BLOCK WALL
- PRIMARY ENTRY/WALL
- SECONDARY ENTRY/WALL
 - (RESIDENTIAL)
- NEIGHBORHOOD ENTRY/WALL
- COMMERCIAL ENTRY

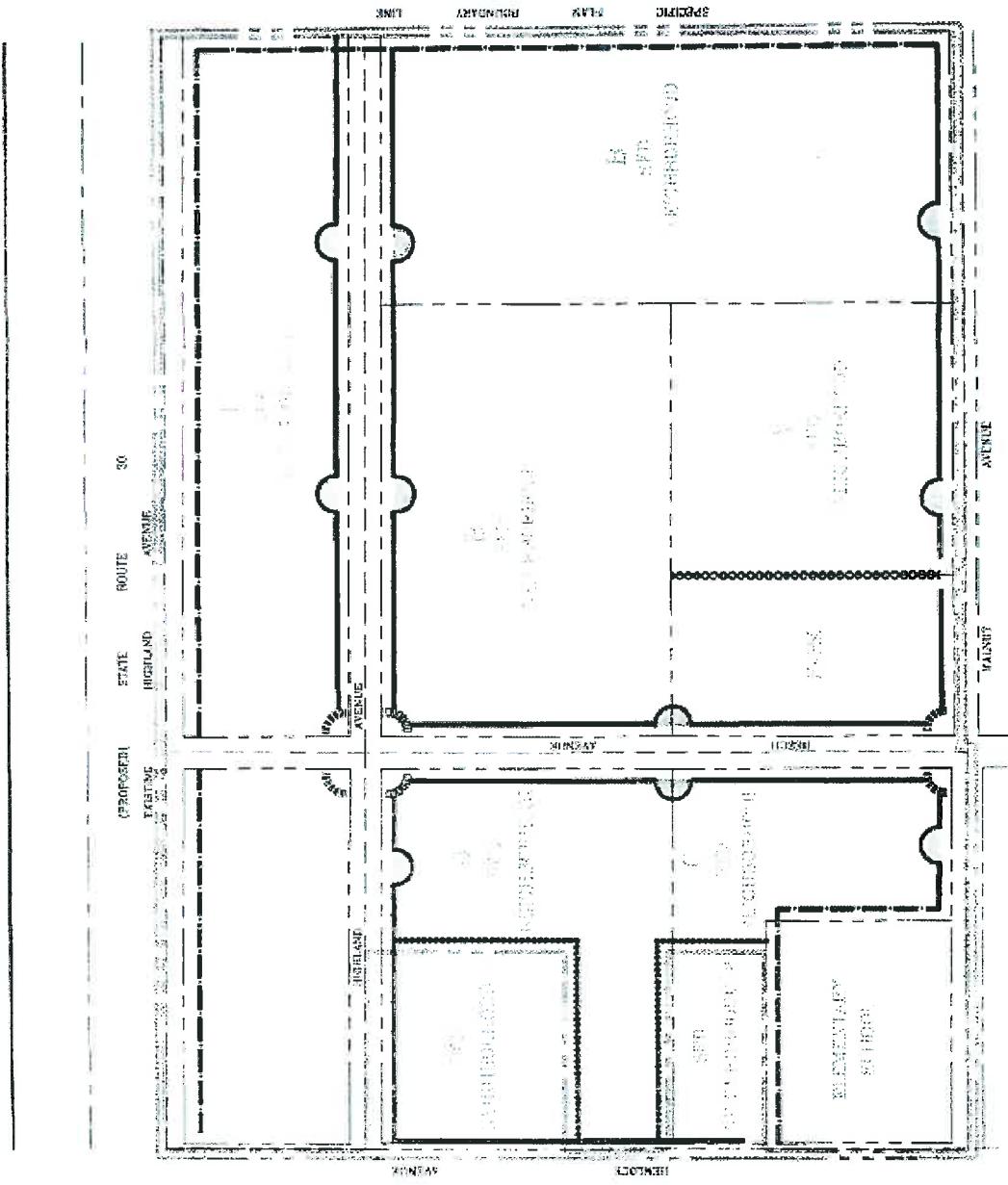


Exhibit 41

California LANDINGS

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4.6 WALLS AND FENCES

4.6.1 Introduction

Walls and fences are an important part of the California Landings Specific Plan aesthetic character. Locations, types, materials and placement shall be adopted with this document. Exhibits 41 and 42 show conceptual locations and elevations of walls and fences.

1. **Entry Wall:**

Entry walls serve as a backdrop to the entry monuments. The entry has stone-clad pilasters and the wall is constructed of split face block with cap, and treated with graffiti resistant coating. Wall height is six (6) feet (See Exhibit 42).

2. **Community Theme Walls:**

Community theme walls will serve to provide privacy and noise attenuation where necessary along residential neighborhoods. These split face block with cap walls treated with graffiti resistant coating will be six (6) feet in height, (see Exhibit 42) with pilasters in the same material at 100 foot on center.

3. **Perimeter Walls:**

Perimeter walls will serve to buffer residential uses within several low visibility locations through the project, and will provide privacy and security, both physically and visually. The perimeter wall will consist of six (6) foot high masonry walls treated with graffiti resistant coating (see Exhibit 42).

4. **Park Walls:**

Park walls will serve to provide enclosure and identity for the park areas while providing a level of security to residential areas. Park walls will be split face block wall six (6) feet in height treated with graffiti resistant coating (see Exhibit 42).

5. **Residential Yards:**

All walls will be of masonry construction or be constructed per City's Ordinance. (see Exhibit 42).

4.6.2 Guidelines

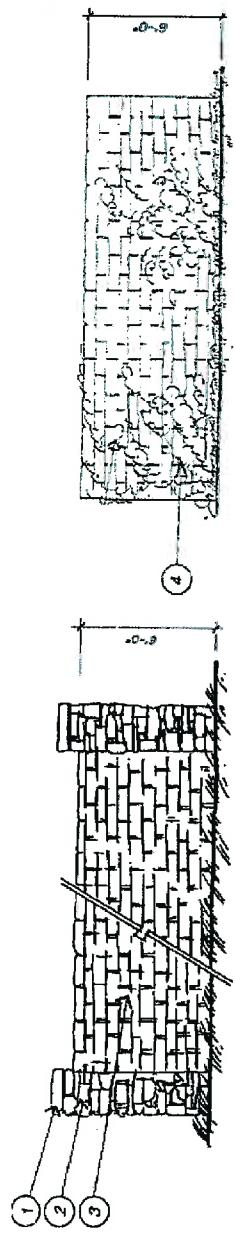
1. Community theme walls shall be installed as the road section the front on is constructed.
2. Perimeter walls and park walls should be constructed as the planning area they border is developed.
3. Entry walls will be backdrops to primary and secondary entries.
4. There will be no fence, wall or obstruction more than thirty inches above street or street curb level within the clear sight triangle (measure thirty (30) feet in both directions on the property line from the corner of two streets), or obstructing views for entry and exit of off-street driveways or multi-parking areas.
5. All single family residential tracts requiring a block or concrete theme fence will have a graffiti resistant coating applied to the section accessible to the public. The coatings will be approved by the Police Department or City Building Official. Commercial building walls shall have a graffiti resistant coating applied up to ten feet when building is accessible to the public, and on perimeter walls on both sides if public access exists. To help deter graffiti, vines which can be quickly established and provide maximum wall coverage may be planted.

WALL & FENCE ELEVATIONS

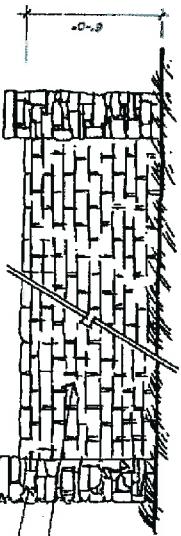
LEGEND:

1. CONCRETE CAP
2. STONE PILASTER*
3. DECORATIVE SETT FACE BLOCK WALL
4. VINE PLANTING
5. MASONRY PILASTERS AT 100' ON CENTER
6. DECORATIVE WALL

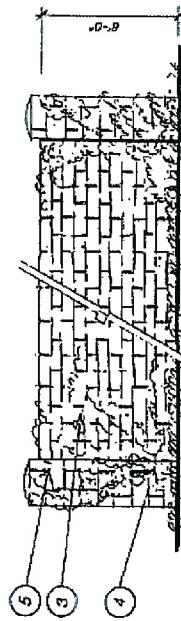
*NOTE:
CULTURED STONE WILL BE USED
FOR ALL STONE PILASTERS.



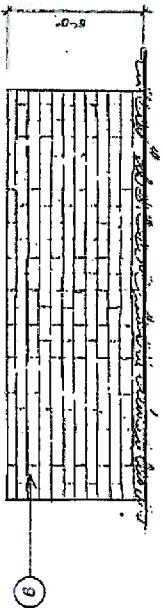
Park Perimeter Wall



Entry Wall



Community Theme Wall



Perimeter Wall/Decorative Block

Exhibit 42

Kaufman
LANDINGS

Kaufman & Broad 

4-45

810225-20 - CH-14

4.7 GRADING

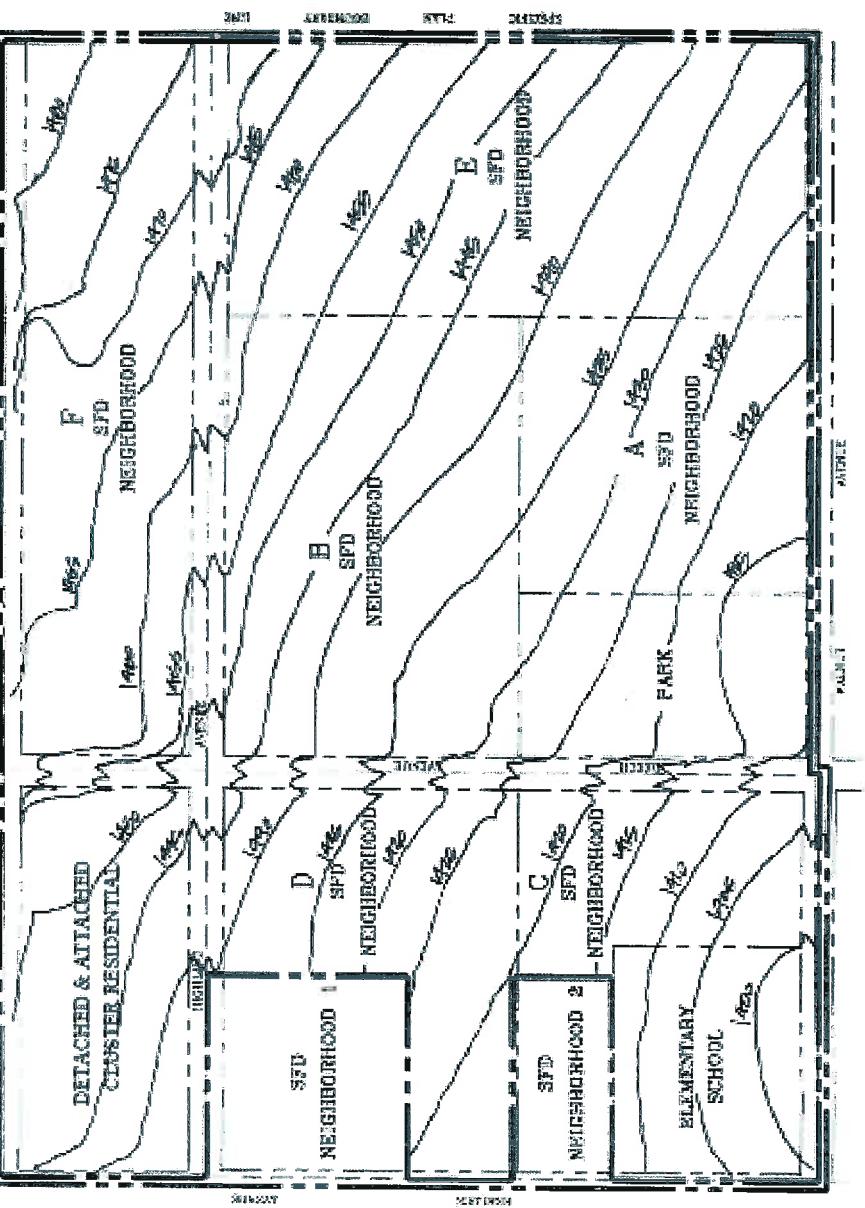
The existing topography on the project site and the surrounding area slopes generally to the southwest. As a result of the mild slopes, no "hillside" or significant grading operation will be required for the site. Building lot grades will generally be in the 1% to 2% range except at the front yards where slopes may be as great as 10% at the transition to the back of the sidewalk. Elevation transitions between lots will not exceed a 2:1 slope unless alternative is approved by City Engineer. Note: On-site grading not in the "public right-of-way" is not under the City Engineer's jurisdiction; it is under the jurisdiction of Building and Safety and therefore the Building Official. Conceptual Grading Plan is illustrated in Exhibit 43 (For storm drainage see Exhibit 50, page 5-13).

In most areas, the proposed grades will be within 2 to 3 feet of existing ground. At the east boundary and at the interface with the two out-parcels, the grading will generally match existing grades. Where elevation differences cannot be avoided, short retaining walls may be constructed. The site earthwork is anticipated to be balanced on-site with minimal import or export.

The existing buildings are anticipated to be demolished and removed off-site to an approved disposal site. The existing asphalt pavement from the runway and other paved surfaces will be broken up and incorporated into the site fill soils, if permitted by the soils engineer.

The alluvial soils are sands and gravel mixed with cobbles. It is anticipated that the alluvial soils will need to be overexcavated and recompacted to provide adequate foundation bearing. Grading will follow the recommendation of a registered geotechnical engineer. Excavation, filling and compaction shall be performed in accordance with local ordinances, building code and engineering practices.

CONCEPTUAL GRADING PLAN



SOURCE:

ALLARD ENGINEERING

NOTE: SEE EXHIBIT 50 FOR
STORM DRAINAGE.

Exhibit 43

California
LANDINGS

Kaufman & Gross

4-47



4.8 SIGNAGE

(See Development Regulations 6.3 and 4.4 Landscape Plan)

4.9 PARKING

(See Development Regulations 6.3 and Table 3)

4.10 LIGHTING**4.10.1 Lighting Guidelines**

A new community area requires a warm, simple lighting geared to its distinctive character. Each light should be attractive to look at during the day as the pole, base and light add another dimension to the urban scene. The lamp elements should not become an important sculptural element on the landscape. Fixtures should be uniform and unobtrusive. All exterior residential, commercial and parking lot lighting will comply with police department lighting standards. No low pressure sodium lighting will be permitted.

1. Building Exterior Lighting

The exterior lighting of commercial and public use buildings is intended to give full time visual appreciation and to attract attention to these buildings during the night hours. Lighting will complement architectural style.

2. Street Lighting

Streets and intersections will be well lighted based on city standards. The maximum height of street lights (with the exception of safety lights at intersections), should be approximately twenty-five (25) feet. Intersections might have increased wattage for definition and to mitigate automobile/pedestrian conflicts. Street lights will conform to overall project theme and city standards.

3. Parking Areas

Parking areas should be illuminated with soft light from small fixtures or lights from hidden sources. In order to promote a pedestrian environment, the elevation of lighting standards in parking areas or their periphery shall not exceed twenty (20) feet in height.

4. **Walkway, Trail and Path Lighting**

Low, pedestrian level lighting may be installed at selected intersections along walkways, trails and paths, in accordance with overall landscaping and architectural theme.

5.0 PUBLIC FACILITIES PLAN

5.1 INTRODUCTION

The provision of adequate infrastructure and public facilities is essential for the development of a master planned community. This Public Facilities Plan for the California Landings development outlines a program to ensure that necessary infrastructure and public services are provided consistent with development needs, and are efficient in design, implementation and installed in a timely manner. This plan identifies the proposed land uses and provision of related public facilities required to service the California Landings project.

5.2 WATER SOURCES AND SUPPLY

Off-site Water

The project is within the Fontana Water Company service area. The proposed water systems for the project will be designed and constructed by Fontana Water Company in accordance with the company's standards. The District has adequate water supplies for water service to the site.

The site is located at the western fringe of the Fontana Water Company service area. The area surrounding the project site is relatively undeveloped. As a result, some existing water facilities will need to be upgraded and water mains extended to provide adequate fire protection and domestic water service to the site.

The project site is located in the "Highland" pressure zone which is fed by a reservoir at Citrus and Summit Avenues. Water from the reservoir is fed towards the project area by a 20 inch diameter line in Citrus Avenue. A 12 inch line tees off of the 20 inch line at Baseline Road and extends west to Lime Street. There is also a 12 inch line in Highland Avenue immediately east of Beech Avenue.

Water service for development of the project may be accomplished by bringing a new 12 inch water main from the existing 20 inch line in Citrus Avenue westward down Curtis to Maloof, then south on Maloof to connect to the existing 12 inch line at Maloof and Highland. Project water service will then go west through the existing 12 inch main in Highland Avenue and will turn southward to continue down Beech Avenue as a new 12 inch line through the California Landings project. This new 12 inch line in Beech will turn west at Baseline Road and will ultimately connect to the end of the existing 12 inch line in Baseline Road, approximately 1850 feet west of Beech Avenue.

These improvements will provide adequate fire and domestic water needs for the project. The developer intends to enter into master water agreements with the Fontana Water Company for off-site construction of infrastructure through the execution of a main extension contract.

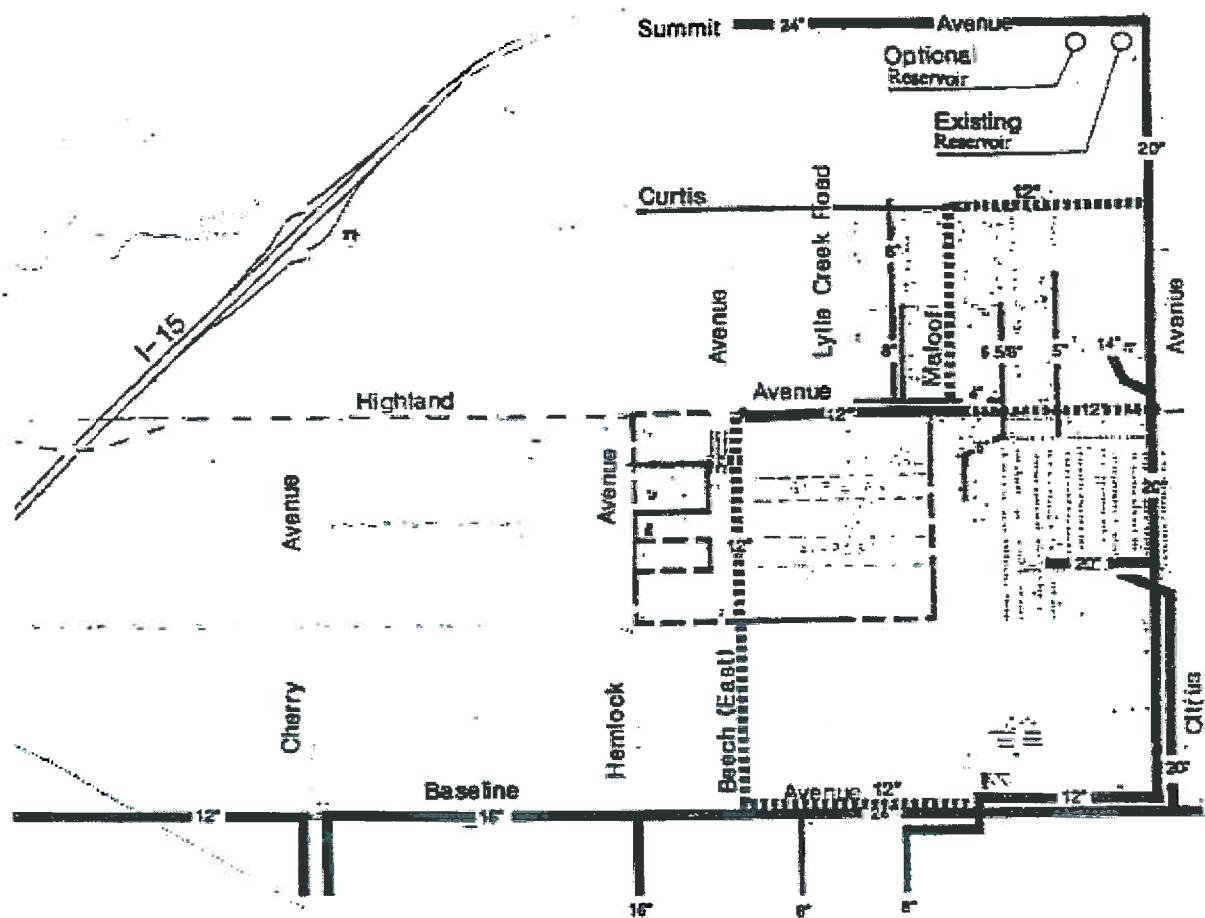
A new reservoir will be required to ultimately serve the site. It will be located north of Duncan Canyon Road near the I-15, or would be at Summit and Citrus.

On-site Water

Within the project site, it is anticipated that 12 inch mains will be constructed and/or retained in the new Highland Avenue and Beech Avenue.

For the initial phases of the project, the water needs of the project may be satisfied with a single 12 inch main in Beech Avenue connected at either Highland and Maloof or Baseline and Lime. The District will require that the initial phases have an internal loop in each neighborhood planning area and tie to the main at a minimum of two locations.

The local water line network within the individual neighborhoods will consist primarily of 8 inch lines. On short cul-de-sacs without fire hydrants, 6 inch lines may be utilized. All lines will be sized to provide adequate fire flows, as determined by the Fire Marshal. The sizing of water lines and hydrants shall take into consideration the enhanced fire protection provided by fire sprinklers required by City Code to be installed in all dwelling units.



PROPOSED WATER INFRASTRUCTURE

LEGEND

- EXISTING WATER LINE
- PROPOSED WATER LINE
12" & LARGER
- PIPE DIAMETER
- RESERVOIR

Exhibit 44

California
LANDINGS

Kaufman & Broad ▲



EWI

WATER PLAN

LEGEND:

PROPOSED 12" WATER LINE
PROPOSED 8" WATER LINE
EXISTING 18" WATER LINE

SOURCE:

MECHANICAL ENGINEERING

MINUTE.

6" WATER LINE WHICH
WALNUT IS NOT NECESSARY
TO PROVIDE. WATER SERVICE TO
CALIFORNIA LANDINGS AND WILL BE
CONSTRUCTED WITH THE ASSOCIATED
FROZEN FLUKE ROADWAY
IMPROVEMENTS.

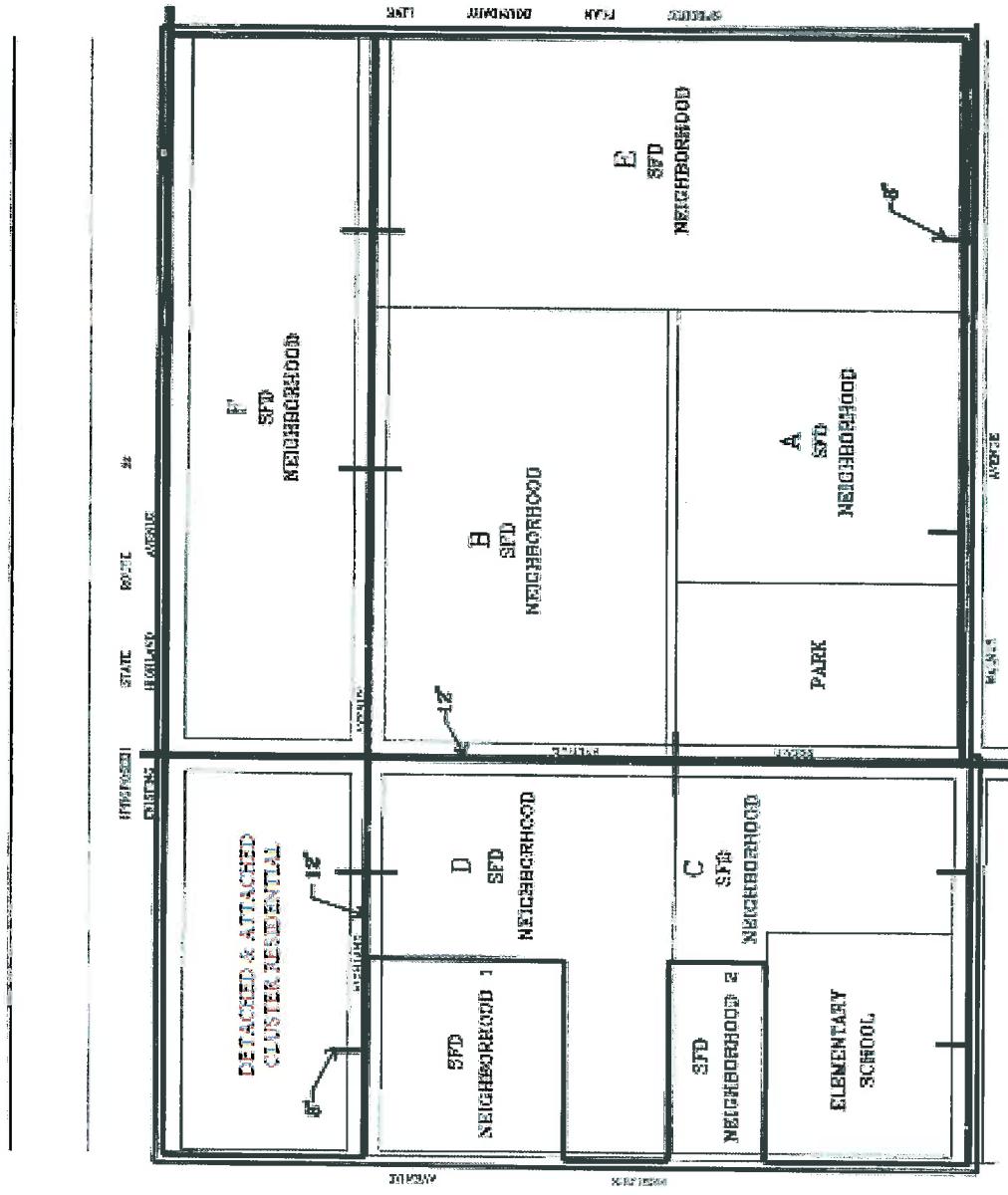


Exhibit 45

California LANDINGS

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5.3 WASTEWATER AND SANITARY SEWER

Off-Site Sewer

Sewer service for the project is provided by a joint effort by two agencies. Chino Basin Municipal Water District (CBMWD) provides domestic wastewater treatment and disposal, non-reclaimable wastewater collection and disposal, and operation of the interceptor sewer lines. The City of Fontana owns and maintains the local sewer collection system. All sewer lines within the California Landings project and the of-site connection to the existing trunk sewer at Baseline Road are local sewer lines and will be designed and constructed in accordance City of Fontana standards. At this time, there is sufficient sewer capacity in both the collection lines and the wastewater treatment plant to serve this project.

In 1990, the 21 inch Etiwanda Trunk Sewer was constructed to the intersection of Beech Avenue and Baseline Road. According to the City's Master Sewer Plan, sewer service to the project will be provided by extending a 15 inch trunk sewer approximately 2,640-feet north from the existing 21-inch Etiwanda Trunk Sewer. As a part of the Sewer Master Plan, the 15 inch line is to be extended north to the northerly tract boundary (existing Highland Avenue). The trunk line will be extended north of the project site by others as the properties to the north develop. The developer intends to enter into reimbursement agreements with the City for off-site construction and pipe oversizing. This agreement will be included in the development agreement.

On-Site Sewer

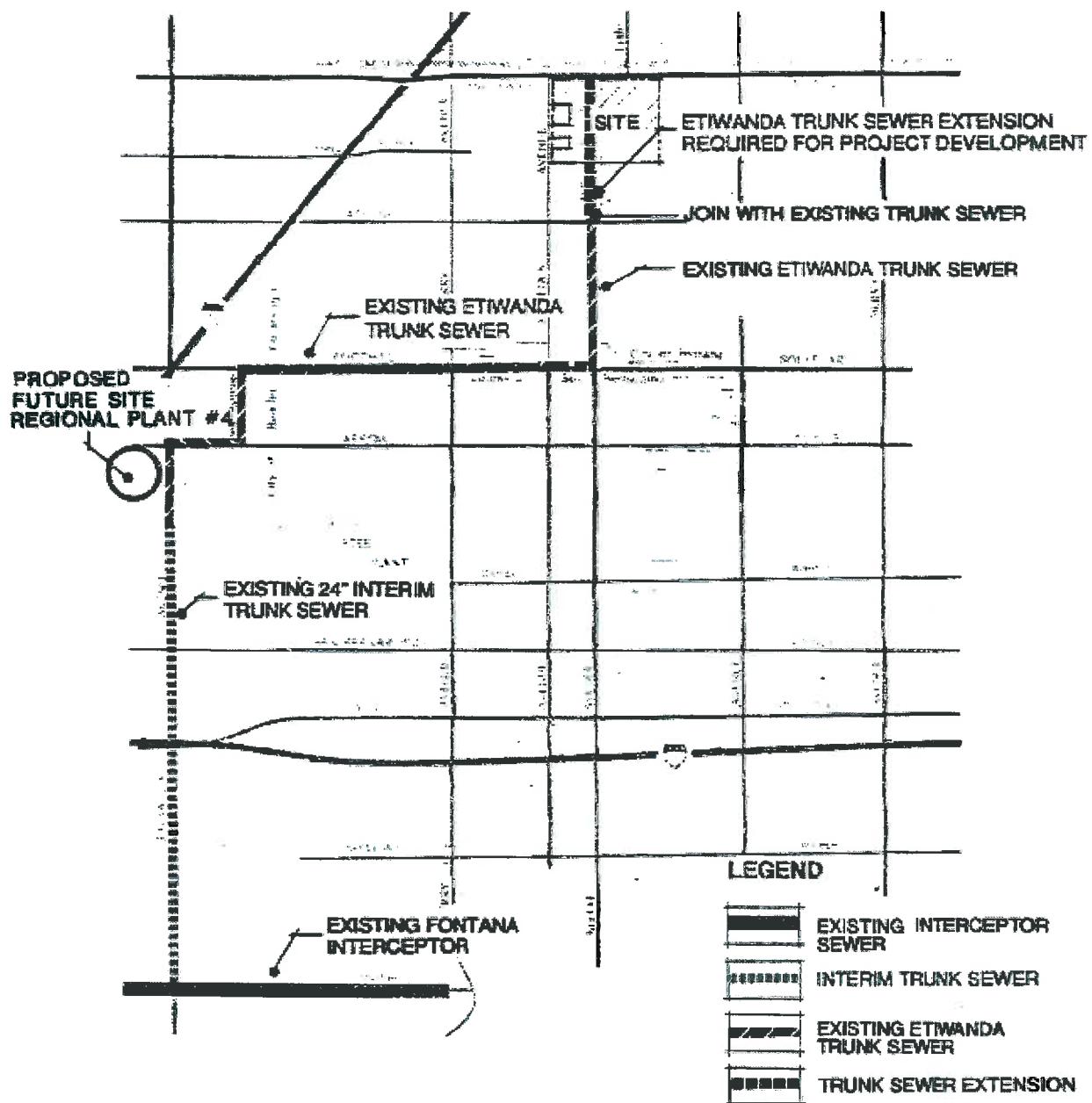
The 15 inch sewer trunk in Beech will be the spine for the on-site sewer lines for the individual neighborhoods. The natural terrain of the project site slopes in a southwesterly direction. Therefore, the neighborhoods east of Beech Avenue can be served by sewer without difficulty. A line extending into the east local collector will serve Neighborhood A. A line along a southerly east-west street through Neighborhood B will be extended to serve Neighborhoods B, E and F. As the properties east of California Landings will be served by another trunk sewer just south of the southeast corner of the site, it is unnecessary to construct a separate sewer line in Walnut Avenue east of Beech Avenue.

The neighborhood and commercial parcel west of Beech Avenue are a little more difficult to serve with sewer since the natural terrain slopes to the southwest. The areas north of the local collector and the areas within 200-300

feet of Beech Avenue can be served easily by aligning the sewer generally along the contour lines. However, the southwesterly corner of the site is about 10 feet lower than Beech Avenue at the south tract boundary and will require some extra care to assure service to that area. By deepening the trunk sewer and filling the southwesterly portion of the site, the sewer can be drained to the trunk line in Beech Avenue.

The sewer will not be extended into existing Highland Avenue east of Beech Avenue. With Highland Avenue being the upper edge of the sewer tributary area after construction of the freeway, there is no need for any local sewers in Highland east of the project. The only sewer lines serving properties north of the future freeway and cross the freeway will be the trunk sewer lines such as the trunk sewer in Beech Avenue.

The trunk sewer line in Beech Avenue, in accordance with the Sewer Master Plan, will be 15 inches in diameter from Baseline to Walnut, and 15 inches from Walnut to Highland. All other sewer main lines are anticipated to be 8 inches diameter. The trunk sewer, if it is deepened, is anticipated to not be in excess of 20 feet deep.



SEWER INFRASTRUCTURE



Exhibit 46
California
LANDINGS

Kaufman & Broad ▲

SEWER PLAN

LEGEND:

15" SEWER
8" SEWER

SOURCE:

ALLARD ENGINEERING

Exhibit 47

California LANDINGS

Kauffman Broad

6

Date	Number of Patients
10/22/05	800
10/23/05	400
10/24/05	200
10/25/05	150
10/26/05	100
10/27/05	80
10/28/05	60
10/29/05	50
10/30/05	40
10/31/05	30
11/01/05	20
11/02/05	15
11/03/05	10
11/04/05	8
11/05/05	5

5.4 STORM DRAINAGE

Off-Site Storm Drainage

The project site slopes southwesterly at approximately two percent slope. The tributary area upstream of the site encompasses an area of approximately 760 acres. An existing earth ditch located along the proposed Beech Avenue alignment carries off-site and on-site runoff from areas to the east. The ditch runs south through the site to Baseline Road where the water flows west on Baseline Road or overflows to the south. For the western portion of the site, the runoff is carried on Hemlock Avenue.

The site is currently covered with impervious surfaces on approximately 20% of the overall site. Most of the impervious surface is the asphalt runway of the former airport operation with the remaining areas consisting of old airport buildings, farm buildings, and residences on the site. This impervious surface increases the runoff from the site as compared to an undeveloped site. The removal of the old runway will cause the runoff from the site to be decreased from what exists today. Therefore, development of the initial phases of the project will not result in significant increases in the runoff from the site.

Storm drainage master planning is a joint responsibility of the City of Fontana and the San Bernardino County Flood Control District (SBCFCD). SBCFCD is responsible for the regional channels, debris basins, and detention facilities. The City is responsible for the acceptance, maintenance and master plan design of the local drainage collection systems, local detention basins and the infrastructure lines carrying runoff to the County regional facilities.

The City of Fontana has prepared a Master Plan of Drainage for Fontana. The project site is within the Master Plan area. The master planned drainage facilities are designed for a design capacity sufficient to accommodate a 25-year frequency storm within the storm drain pipe system. Flows for a 100-year frequency storm which are in excess of the capacity of the pipe system are calculated to be fully contained within the downstream street right of ways.

No master planned infrastructure drainage facilities exist in close proximity of the project. The nearest City master planned storm drain facility to serve the project is the double 10' x 8" reinforced concrete box culvert (RCB) located in Baseline Road approximately 6000 feet west of Hemlock Avenue. According to the City master plan, this storm drain is planned to be extended east to Mango Avenue. Currently, stormwater runoff in the area is intercepted by an existing earth ditch along the north side of Baseline Road.

The ditch currently runs from the end of the existing RCB to Live Oak Road.

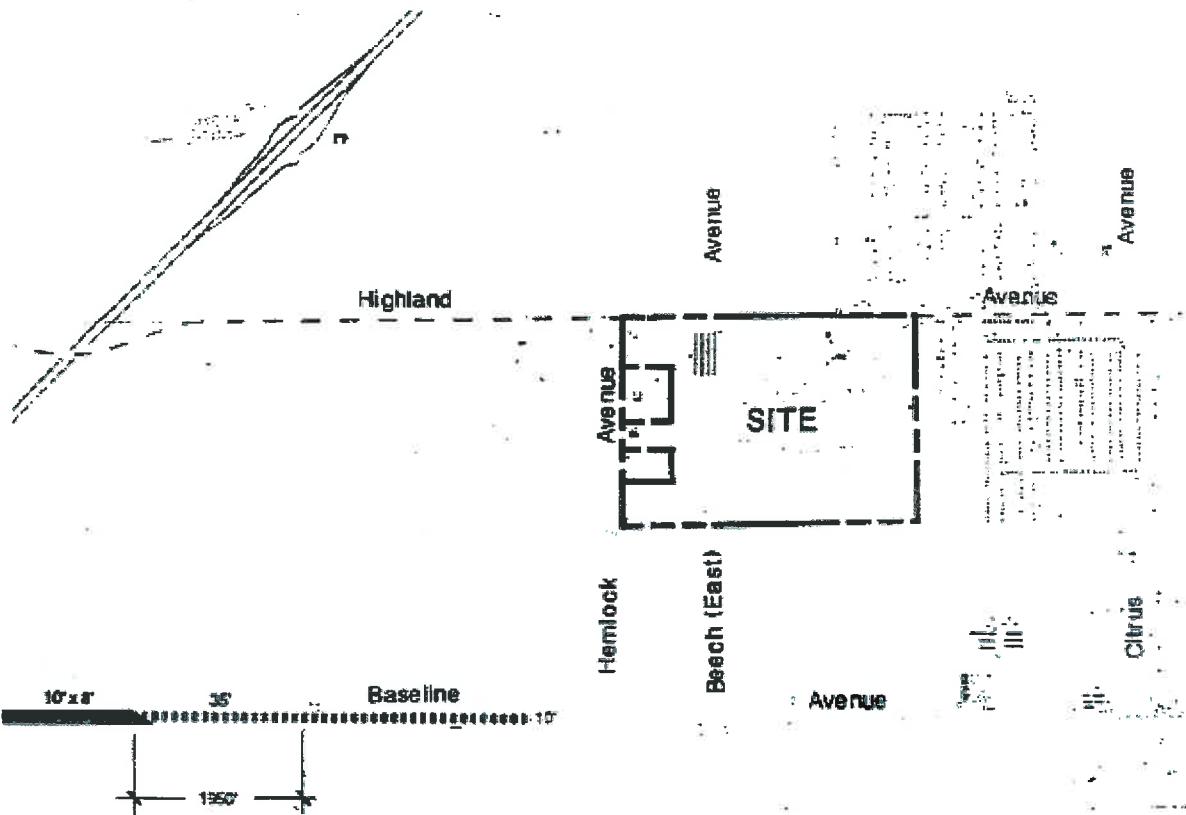
The master planned off-site storm drainage improvements to serve the project site consist of a 66" pipe along Hemlock from Walnut to Baseline Road (see Exhibit 49). Also, given the fact that over 20% of the California Landings site is currently covered with asphalt pavement and buildings, the site will actually produce less runoff after those impervious areas are demolished and the site is graded. Therefore, during the initial phases of the project, extension and improvement of the existing earthen ditch along Baseline Road to Hemlock Avenue will be sufficient to handle the developed flows from the site. If needed, on-site or off-site detention will be provided to satisfy the City's and SBCFCD requirements for increased runoff mitigation.

Special care is required in the design of the basins to analyze the effect of all out-flows on downstream property owners. Low flow storms (eg. 2, 5, 10, 25, and 50 year) need to be specifically addressed as they may have a larger impact downstream than a 100 year storm. Flows exiting a detention basin need to be designed to a point of discharge to a City or County Master Planned Facility. This will insure that there are no downstream impacts resulting from the use of the basins.

In addition to the City drainage improvements, the project must also satisfy SBCFCD requirements for detention. Due to an agreement with Riverside County to limit drainage flows into Riverside County, SBCFCD must detain flows from San Bernardino County. SBCFCD is planning for several regional detention basins in the area. Until the detention basins are constructed, SBCFCD requires that all developments in the affected areas provide on-site detention to reduce the runoff flows to 90% of the existing runoff from the site.

On-Site Storm Drainage

On-site project storm drains are expected to be constructed along Walnut and Beech and the local collector. A system of on-site interim detention basins are proposed for the project if needed to mitigate increased runoff. Temporary basins are proposed as development progresses to allow for logical flood protection. These basins are intended to reduce the peak runoff and minimize overtaxing the downstream facilities. Interim detention basins will require a general plan amendment. In addition, temporary berms will be utilized along Highland Avenue due to the timing and construction of drainage facilities along State Route 30.



LEGEND

-  EXISTING BOX CULVERT & RCB
-  EXISTING EARTH CHANNEL
-  WIDTH OF FACILITY

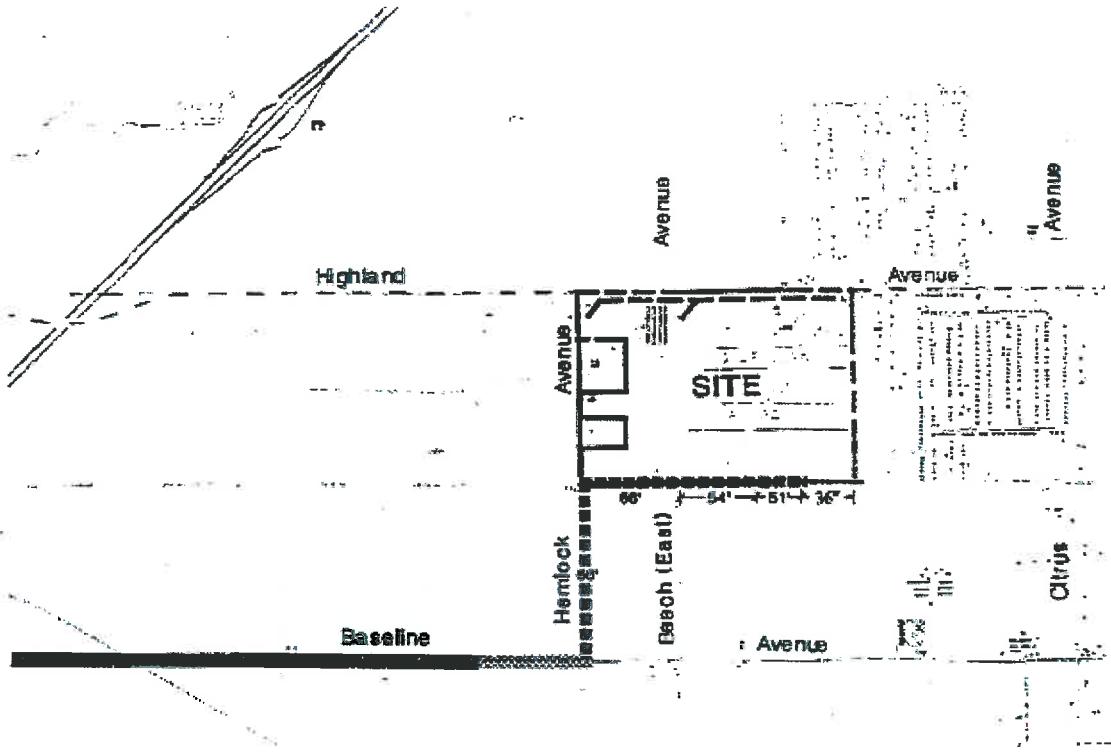
EXISTING MASTER STORM DRAINAGE FACILITIES



KAUFMAN & BROAD

Exhibit 48
California
LANDINGS

Kaufman & Broad ▲



LEGEND

- EXISTING INTERIM DITCH
- PROP. INTERIM DITCH EXTENSION
- PROP. STORM DRAIN PIPE
- PROP. INTERIM DITCH

PROPOSED STORM DRAINAGE INFRASTRUCTURE



Exhibit 49

California
LANDINGS

Kaufman & Broad

STORM DRAINAGE PLAN

POTENTIAL INTERIM BERMS TO PROTECT
RESIDENTIAL NEIGHBORHOOD FROM SHEET FLOWS

ROUTE
(PROPOSED)

REGRADE EXISTING
DITCH TO FLOW

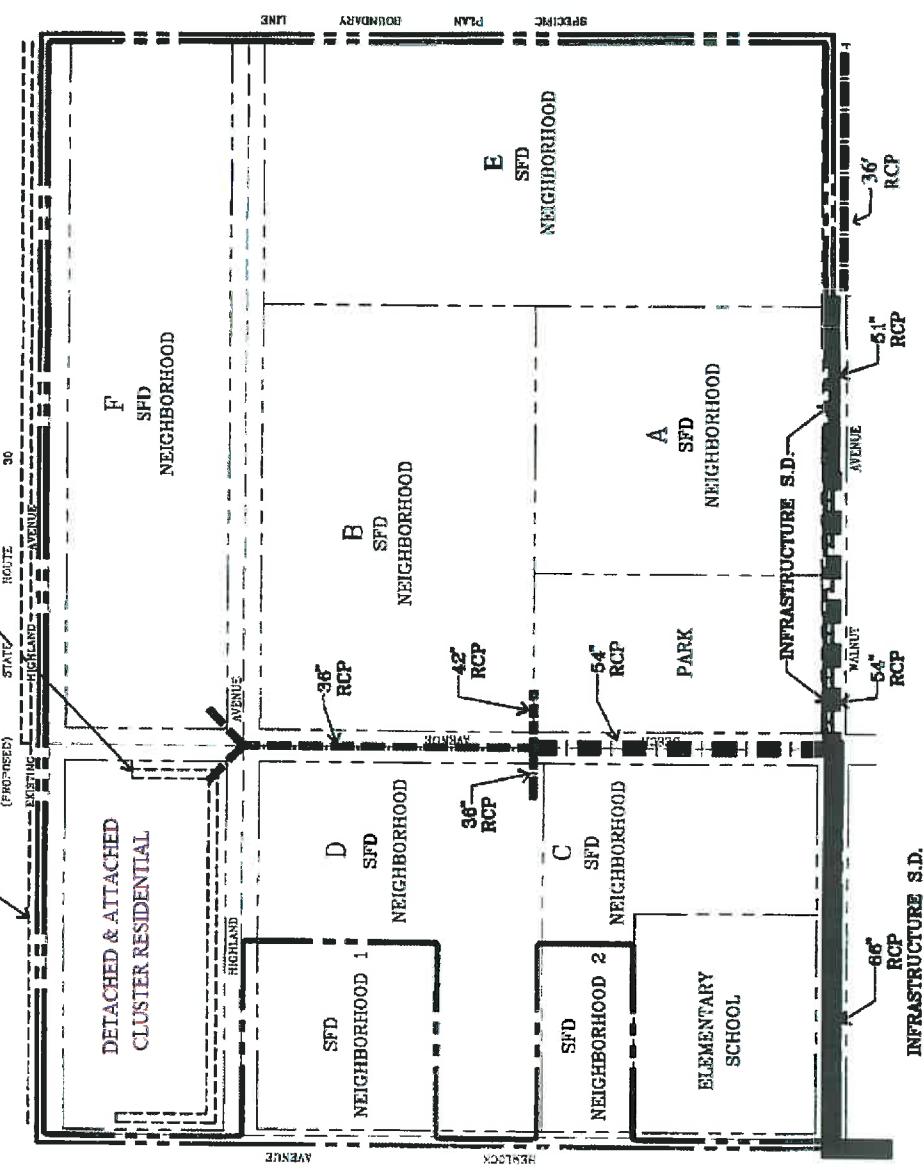


Exhibit 50
California
LANDINGS
Kaufman & Broad

5.5 PUBLIC SERVICES

Fire

The San Bernardino County Central Fire Agency provides fire protection services to the project site from Fire Station #78 with an estimated response time of one minute from its Citrus Avenue location. The site is also served by Station #73 on Arrow Boulevard. These stations are staffed with three full-time (24 hours) firefighters.

Medical/Ambulance

Mercy Ambulance Service provides emergency medical service with 78 Emergency Medical Technicians and Paramedics to the areas of Fontana, Bloomington, Rialto, Rancho Cucamonga, Ontario and Chino. Average response time in Fontana to an emergency call is eight (8) minutes. The nearest hospital, Kaiser Hospital, at 9961 Sierra Avenue is approximately a seven mile drive from the site.

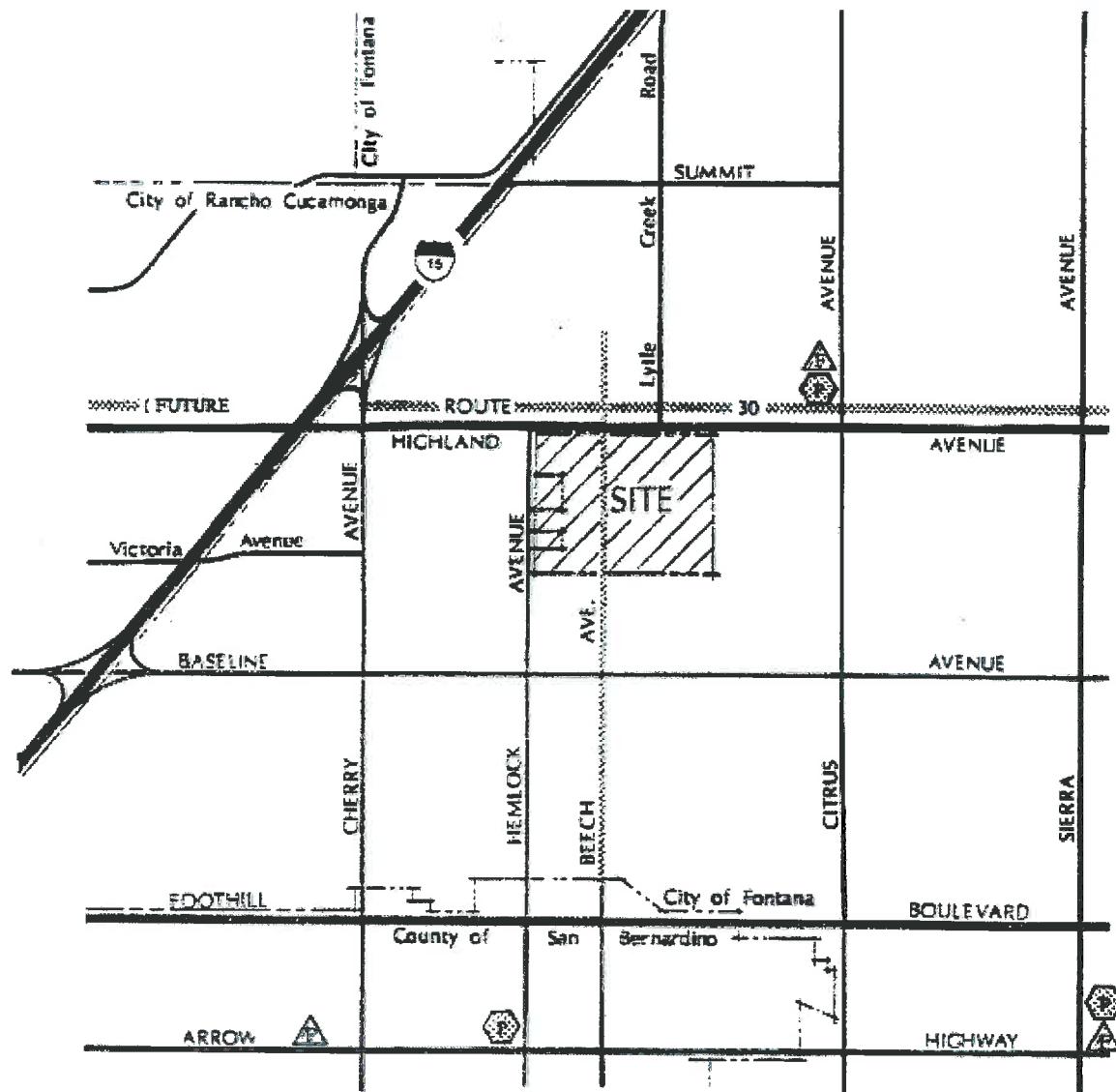
Police

The Fontana Police Department provides law enforcement service for the City of Fontana with a current force of 115 sworn and 53 non-sworn personnel. The department currently has a total of 66 police vehicles including black and white units, motorcycles, trucks and vans. They operate in cooperation with surrounding law enforcement agencies under the "State Mutual Aid Pact". Police Headquarters is located on Upland Avenue approximately 4.5 miles from the project site. Three police contact stations and a substation are proposed within the North Fontana area (see Exhibit 51).

Schools

Adequate school facilities mitigation will be provided. The Landings site is located within the Etiwanda, Chaffey, and Fontana Unified School District. An on-site school facility is to be included in the Etiwanda School District. However, the project site is currently bisected by the Etiwanda and Fontana Unified School District boundaries. The boundary has been modified to conform with the neighborhood boundary alignments. The desired goals of the school districts is not to bisect planning areas. An exhibit showing the school district boundaries and existing facilities is included (see Exhibit 52).

The Etiwanda School District Board of Trustees has selected the California Landings project as a possible site for a future elementary school site (see Exhibit 53). There will Possibly be a minor school district boundary adjustment prior to the issuance of occupancy permits.



LEGEND:

- ▲ FIRE STATION
- POLICE STATION

**PUBLIC
FACILITIES MAP**

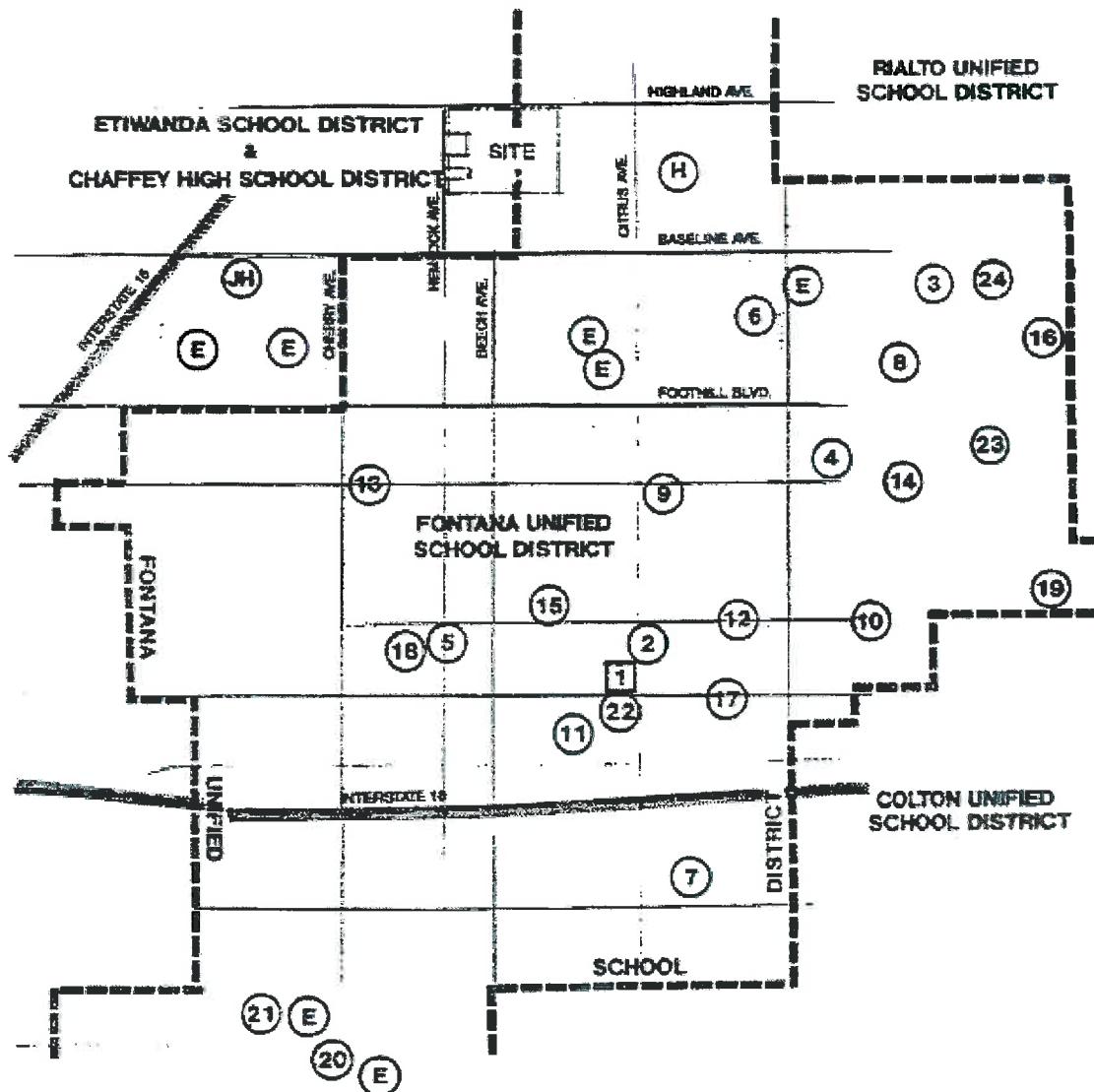


1991

Exhibit 51

California
LANDINGS

Kaufman & Broad ▲



FONTANA UNIFIED SCHOOL DISTRICT

1. Administration Building

HIGH SCHOOLS

2. Fontana High School

JUNIOR HIGH SCHOOL

- 3. Alder J.H.S.
- 4. Fontana J.H.S.
- 5. Paquidia J.H.S.

ELEMENTARY SCHOOLS

6. Juniper E.S.
7. Jurupa E.S.
8. North Tamarind E.S.
9. Oleander E.S.
10. Palmetto E.S.
11. Poplar E.S.
12. Randall-Pepper E.S.
13. Redwood E.S.
14. South Tamarind E.S.
15. West Randall E.S.
16. Virginia Primrose Special School

17. Cypress E.S.

- 18. Live Oak E.S.
- 19. Maple E.S.
- 20. Southridge Middle School
- 21. Chappatal E.S.
- 22. Alternative Education
- 23. Birch Continuation School
- 24. Locust E.S.

PROPOSED SCHOOLS

E Elementary
JH Junior High
H High

EXISTING SCHOOL SITES



**California
LANDINGS**

Kaufman & Broad A

5-17

SCHOOL SITE
AMENITY ALTERNATIVE

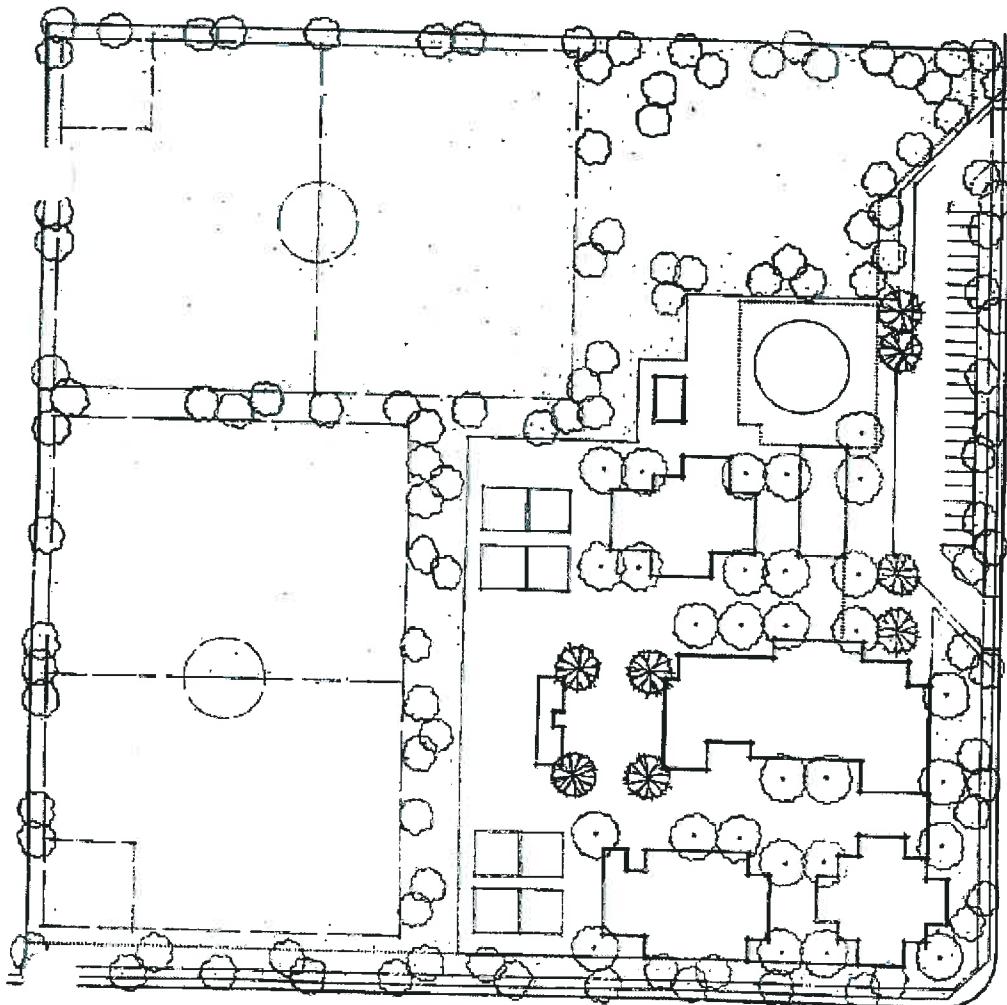


Exhibit 53

California
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5-18

6.0 SPECIFIC PLAN IMPLEMENTATION

6.1 INTRODUCTION

Implementation of the California Landings Specific Plan is accomplished through a set of detailed Development Regulations, Standards, and Procedures. The California Landings Specific Plan provides several benefits to the City of Fontana which justifies the adoption of these regulations and standards. These benefits include:

Contributing to a balance of land uses, including a variety of housing opportunities, community support facilities such as parks and shopping areas, consistent with the City's General Plan providing for a balanced community.

Enhancing property values on the site and on adjacent lands through substantial infrastructure and other capital improvements.

Providing for a comprehensive Master Planned Community on 223 acres consistent with the goals of the City's proposed Land Use Element by the consolidation of parcels.

Helping to create a new and positive image for the City of Fontana through the development of a quality Master Planned, residential and mixed use Planned Community on a site currently occupied by abandoned structures (i.e. airfield and chicken ranch).

Incorporating a comprehensive landscape plan intended to provide identity and character to the project, creating a visually pleasing community image, enhancing the transition between open space areas and residential/business areas with the use of edge treatments and incorporating a pedestrian/ bicycle trail system to link the land uses within the community.

Establishing a community statement by treating major community entrance/access point elements with a series of community monumentation, enhanced intersection paving materials, lighting and plant materials to identify entry into the community and neighborhoods as well as to enhance the North Fontana image.

Providing a desirable development character which is compatible with existing and proposed development in the surrounding neighborhood.

Achieving a balanced community which improves the quality of life and meets the needs of a variety of age groups, and life styles.

6.2 PHASING

The project will be constructed in multiple phases (see Exhibit 54). The project phasing will be dependent on a variety of factors such as market demands, and the logical extension of on-site services and infrastructure.

The first consideration for any project development is the construction of any necessary off-site infrastructure services to the project. The primary infrastructure needed to serve the project are sewer, water, storm drainage and streets.

6.2.1 Off-Site Infrastructure

a. Sewer

Sewer service for the project site can be brought to the site by constructing approximately 2,650-feet of 15-inch trunk sewer line from the intersection of Baseline Road and future Beech Avenue to the south tract boundary. The amount of trunk sewer constructed within the site will depend on the paving of Beech Avenue and the phasing of the neighborhood planning areas. In Beech Avenue, the sewer will be constructed prior to construction of asphalt pavement over the sewer line (see Exhibit 46).

b. Water

Water service to the project site needs to be upgraded and expanded to assure proper water service. For the initial phases of the project, a 12 inch water main will be extended to the project site. For the ultimate construction of the project, a second 12 inch water main extension will be required which will create a loop. The existing water line in Highland Avenue will be maintained in its present location.

c. Storm Drain

The storm drain infrastructure does not exist within an economically viable distance from the project site. The project proposes to mitigate increased runoff on-site, which will defer the construction of storm drains improvements along Baseline Road. Ultimate improvements will be built through fair share construction of improvements,

or may be built through a Community Facilities District (CFD) or Assessment District. In the interim, extension of the existing earth ditch along the north side of Baseline Road from Live Oak Avenue to Hemlock and north along Hemlock will suffice to intercept the runoff from a relatively undeveloped drainage area (see Exhibit 48).

The California Landings project will construct master planned drainage facilities in the immediate area of the project. The project will construct the pipe along the south edge of the tract (36" to 66" diameter) in Walnut Avenue and along Hemlock (66" diameter) from Walnut to Baseline Road. A junction/outlet structure will be constructed at the south end of the line in Hemlock to direct the flows into the interim ditch (see Exhibit 49).

Major on-site storm drain lines will be constructed up Beech Avenue and at the interface between Neighborhoods A and C. The line in Beech Avenue will be constructed as a part of the initial phase to intercept off-site stormwater until master planned facilities are constructed to the north.

To facilitate discharge metering, temporary detention basins are proposed within project boundaries. Location and design of the basins will be approved by the City Engineer.

d. Road

The major off-site road construction including right-of-way acquisition for the project is the construction of 2650 feet of Beech Avenue (28 feet of pavement) from the south project boundary to Baseline Road. With the construction of Beech Avenue through the project, this will provide a north-south through street.

PHASING PLAN

LEGEND:

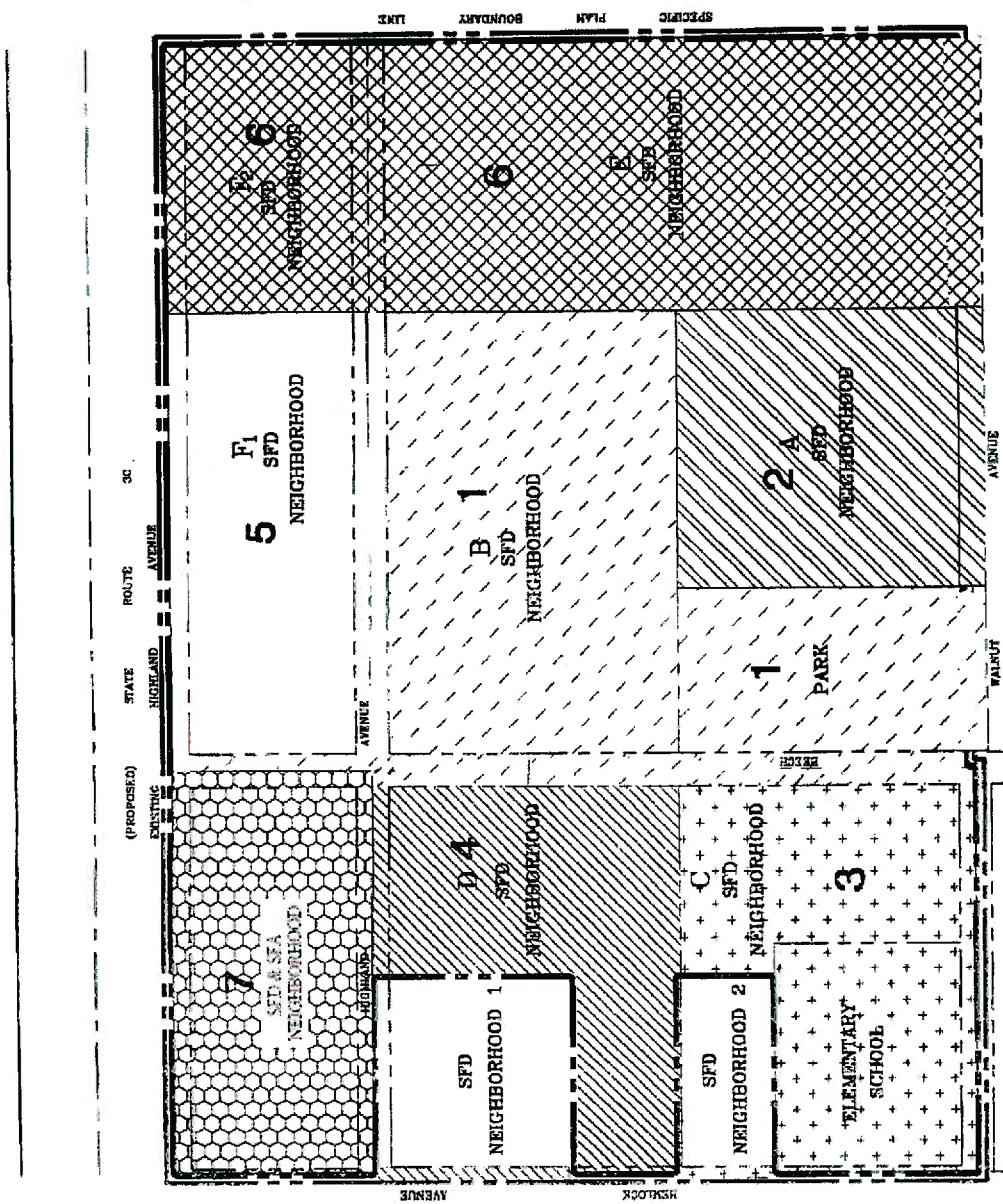


Exhibit 54

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6.2.2 On-Site Project Phasing

The construction and phasing for the individual neighborhood planning areas will be based on the market demand. In addition to the major infrastructure, key components of the project must be constructed to serve any individual proposed neighborhood. The following is a summary of the neighborhood phasing along with key project components which must be constructed as a part of the neighborhood. The construction of any roadway includes utilities (water, sewer and storm drain) under the road ROW. Grading shall be in accordance with approved grading plans.

Based on the location of the sewer and storm drain, Neighborhood B will be the first neighborhood to be constructed followed by Neighborhood A. The sequence of phasing of Neighborhoods C, D, F and E, and the commercial parcel will follow the first two neighborhoods. However, the exact sequence and timing will be determined as the project progresses. The following is the proposed phasing sequence along with a description of the improvements necessary to implement each phase.

Site Description and Required Construction Elements

PHASE 1 Neighborhood B (135 D.U.)

a. Roads

1. Dedicate and construct curb to curb of Beech Avenue from Walnut Avenue to Highland Avenue.
2. Dedicate and construct minimum 28 feet of pavement on Beech Avenue from Highland Avenue north to existing Highland Avenue (Rte. 30).
3. Walnut Avenue (460 feet abutting Park) - dedicate and construct north 36' of roadway.
4. Construct sidewalk and landscape on east side of Beech Avenue from parksite to Highland Avenue.
5. Construct median from park site to Highland Avenue.

b. Water

1. Construct water line in Beech Avenue and 460 feet of water line in Walnut Avenue

c. Sewer

1. Construct sewer line in Beech Avenue.

d. Storm Drain

1. Construct approximately 2,000 ft. of storm drain in Beech Avenue north from Walnut Avenue to Highland Avenue.
2. Construct storm drains in Walnut Avenue east of Beech Avenue (460').
3. Construct temporary detention basin and berms as determined by City Engineer.

e. Grading

1. Grade site and construct interior streets, sewers, storm drain facilities and necessary utilities.

f. Park

1. Dedicate Park land (10.0 acres) to the City of Fontana. Improve park to neighborhood park standards.

g. Primary Entry

1. Construct primary entry monument at southeast corner of Highland and Beech Avenues.

h. Secondary Entry Monumentation

1. Construct Secondary Entry Monumentation at the northeastern corner of Beech Avenue and Walnut Avenue

i. Community Theme Walls

1. Construct community theme walls for Neighborhood B along Beech Avenue.

2. Construct community theme wall along Highland Avenue for Neighborhood B.

PHASE 2 Neighborhood A (88 D.U.)

a. Roads

1. Dedicate and construct northerly 36 feet of roadway of Walnut Avenue from Phase 2 to the south eastern corner of Phase 2.

b. Water

1. Facilities that accompany roads in this phase.

c. Sewer

1. Facilities that accompany roads in this phase.

d. Storm Drain

1. Interior storm drain lines will be extended from the infrastructure storm drain line in Walnut Avenue.

e. Grading

1. Grade site and construct interior streets, sewers, water, storm drain facilities and utilities as necessary.

f. Perimeter Wall

1. Construct perimeter wall along eastern and northern edges of Neighborhood A.

g. Community Theme Wall

1. Construct community theme wall along Walnut Avenue frontage.

PHASE 3: Neighborhood C (53 D.U.)**a. Roads**

1. Dedicate and construct north half of Walnut Avenue from Beech Avenue to Hemlock Avenue.
2. Construct east half of Hemlock Avenue from north boundary of Phase 4 to Walnut Avenue. Construct community theme wall along Hemlock.
3. Landscape west half of Beech Avenue to Neighborhood C.

b. Water

1. Construct as necessary.

c. Sewer

1. Construct 800 feet of sewer line in Walnut Avenue from Beech Avenue westward and extend into the neighborhood for local sewer service.

d. Storm Drainage

1. Modify interim detention basin, if needed, at the southwest corner of the neighborhood and extend storm drain lines into the neighborhood for local drainage collection.

e. Grading

1. Grade site and construct interior streets, sewers, storm drain facilities and necessary utilities.

f. Community Theme Wall

1. Construct a community theme wall along Beech Avenue and Walnut Avenue abutting Neighborhood C.

PHASE 4 Neighborhood D (91 D.U.)**a. Roads**

1. Landscape west half of Beech Avenue adjacent to Neighborhood D.
2. Construct east half of Hemlock Avenue (30 feet of pavement) from south boundary of Phase 4 to Highland Avenue.
3. Construct 600 feet of south half of new Highland Avenue (33 feet of pavement curb to curb) west of Beech Avenue abutting Neighborhood D.

b. Water

1. Construct 600 feet of 8 inch water line in new Highland Avenue.

c. Sewer

1. Facilities that accompany roads in this phase.

d. Grading

1. Grade site and construct interior streets, sewers, storm drain facilities and necessary utilities.

e. Primary Entry Monumentation

1. Construct Primary Entry monumentation on southwestern corner of new Highland Avenue and Beech Avenue.

f. Community Theme Wall

1. Construct community theme wall along Beech Avenue and Highland Avenue abutting Neighborhood D.

PHASE 5 Neighborhood F-1 (89 D.U.)**a. Roads**

1. Dedicate and construct Highland Avenue from Beech Avenue to

eastern tract boundary. Interim connection to existing Highland Avenue.

2. Dedicate and construct east side of Beech Avenue from future Highland Avenue to existing Highland Avenue.

b. Water

1. Construct water line in Highland Avenue

c. Sewer

1. Sewer service for this site will be via the sewer lateral at the southwest corner of the neighborhood which was constructed by a previous phase.

d. Storm Drain

1. Construct as necessary.

e. Grading

1. Grade site and construct interim streets, sewers, storm drain facilities and necessary utilities.

f. Perimeter Wall

1. Construct Perimeter Wall along eastern boundary of Neighborhood F.

g. Community Theme Wall

1. Construct community theme wall along future Highland Avenue adjacent to this neighborhood.

PHASE 6 Neighborhood E (218 D.U.) and Neighborhood F-2 (76 D.U.)

a. Roads

1. Dedicate and construct northerly 33 feet of roadway of Walnut Avenue from Phase 2 to the easterly tract boundary.
2. Dedicate and construct Highland Avenue from Phase 5 to easterly

tract boundary.

b. Water

1. Facilities that accompany roads in this phase.

c. Sewer

1. Facilities that accompany roads in this phase.

d. Storm Drain

1. Interior storm drain lines will be extended from the infrastructure storm drain line in Walnut Avenue

e. Grading

1. Grade site and construct interior streets, sewers, water, storm drain facilities and utilities as necessary.

f. Perimeter Wall

1. Construct perimeter wall along eastern and northern edges of Neighborhood E and F-2.

g. Community Theme Wall

1. Construct community theme wall along Walnut Avenue frontage.

PHASE 7 Cluster Residential

1. Because of the increased traffic loading from residential activity on the site, the remaining north half street and median for future Highland Avenue from Beech Avenue to western boundary will be dedicated and constructed.
2. All other services are in place from construction of prior phases.

LATER PHASES

The above is not intended to be a comprehensive sequence of construction. The required construction elements described above show the minimum improvements

required for the construction of each of the described neighborhoods and parcels. There are many other project elements which have not been described above, such as the construction of the second water main line connection to a feeder. The timing for the completion of these items will be determined as the project nears final design of the various phases. The timing of the construction will be dependent on a variety of considerations such as traffic counts, the rate and extent of development in the surrounding area and construction of off-site adjacent road and utility lines.

The schedule for development for Neighborhoods 1 and 2 (other ownerships) are undefined at this time. Development of these two neighborhoods may be dependent on the construction of services to Neighborhoods C and D. Neighborhoods 1 and 2 can be developed at the same time as Neighborhood D or earlier as approved by City Staff.

6.3 DEVELOPMENT REGULATIONS

The following section and Tables 6.1, 6.1.a, and 6.2 set forth the standards for development of all residential and community mixed use and open space uses within the California Landings Planned Community. This section has been prepared in accordance with California Government Code 65450, et seq. and all applicable development and design standards of the City of Fontana Zoning Code.

Unless otherwise specified in the Specific Plan, the permitted uses and other regulations applicable to the California Landings Planned Community shall be those set forth in the Fontana Zoning Code for those zones identified as equivalent to the uses designed in the Specific Plan, in accordance with the following Table of Equivalent Zoning:

TABLE OF EQUIVALENT ZONING

DESIGNATED USE PER THE SPECIFIC PLAN	CORRESPONDING ZONING CLASSIFICATION AS PER THE FONTANA ZONING CODE
Park	Public Facilities (P-PF)
School Site	Public Facilities (P-PF)
Single Family Detached	Single Family Residential (R-1)
Single Family Detached(min 5,000 sf)	Medium Density Residential (R-2)
Detached & Attached Cluster Residential	Multi-Family Residential (R-3)
Neighborhoods 1 and 2	Residential Planned Community (R-PC)

6.3.1 General Provisions

A. Purpose and Intent

The regulations set forth in this chapter have been established to provide for orderly development of the California Landings Community. These standards provide for the arrangement, development and use of a variety of residential housing types, supporting commercial facilities, and recreational/open space consistent with the intent, purpose and goals of the City's General Plan and which in some cases, exceed the standards contained in the current Zoning Code. Application of these regulations is specifically intended to provide the

most appropriate use of the land, create a harmonious relationship among land uses and protect the health, safety and welfare of the community.

B. General Notes

1. Any land use not specifically covered by the provisions contained herein shall be subject to the regulations of the City of Fontana Zoning Code (Chapter 30) for the underlying (equivalent as set forth in the Table of Equivalent Zoning above).
2. Whenever the regulations contained herein are inconsistent with the regulations of the City of Fontana Zoning Code, the regulations of the California Landings Specific Plan shall take precedence. However, the regulations contained herein shall not take precedence over any health and safety code, uniform building code, or state building code.
3. The maximum number of dwelling units permitted by the California Landings Specific Plan shall be 985. The approximate number of dwelling units within each residential development area is established on the California Landings Land Use Plan. The density, product type and development standards of a residential development area may be transferred to another development area, subject to approval by the Community Development Director, to insure that the transfer does not exceed 10% of the total development area, (does not exceed the density allowed by the General Plan for the Land Use Area), and is consistent with the overall design objectives of this document.
4. The boundaries of individual development areas and acreage are approximate. Precision is limited by the scale at which the Land Use Plan is drawn. Precise planning area boundaries and acreage will be established in conjunction with the submittal and review of subdivision maps. Minor boundary and acreage variations not to exceed 10% may be permitted, subject to approval by the Community Development Director, for conformance with the intent of the Specific Plan without an amendment to this Specific Plan.
5. Should the public school or park site be relocated, the resulting planning area may be developed as R-1-6200 single family detached

housing, in accordance with all existing City standards, codes and regulations.

6. Grading plans for all projects shall comply with the City of Fontana standards. A preliminary geotechnical engineering report and soil engineering investigation showing evidence of recommendations for a safe and stable development is to be submitted with subdivision maps. The recommendations of the engineering geologist and soil engineer shall be incorporated into the grading plan design prior to grading permit approval.
7. All construction shall comply with all provisions of applicable building codes.
8. All terms used in this document shall have the same definitions as provided in the City of Fontana Zoning Code, Chapter 30, Article I, Section 30-3 of the Municipal Code, or the following (to the extent that the terms used in this document do not exist in or are inconsistent with the City of Fontana Zoning Code):
 - a. Building height. The vertical distance measured from the finished first floor elevation to the highest point of the roof surface, if a flat roof; and to mean height level between eaves and ridge for a gable, hip or gambrel roof; provided, however, that where buildings are set back from the street line, then the height may be measured from the average elevation of the finished grade along the front of the buildings.
 - b. Building Setback front. An open space contiguous to the front lot line of a lot and extending from side line to side line thereof and measured between the front lot line and the main structure on the lot.
 - c. Building Setback rear. An open space contiguous to the rear line of a lot and extending from side line to side line thereof and measured between the rear lot line and the main structure on the lot.
 - d. Building Setback side. An open space contiguous to the side line of a lot and extending from the front property line to the rear property line and measured between the side lot line and the main structure.

- e. **Frontage.** All the property fronting on one side of a street between intersecting streets, or between a street and right of way, end of dead-end street or city boundary measures along the street line.
- f. **Grade.** The average of the finished ground level at the center of all walls of a building. In the event walls are parallel to and within five feet of a sidewalk, such ground level shall be measured at the sidewalk. The height of all signs shall be measured at finished grade at the back of sidewalk.
- g. **Lot size.** The total horizontal area included within the lot lines of a lot.
- h. **Lot area, buildable.** The level portion of the net lot area exclusive of impediments which prevent the property owner from building a structure. Means the same as "building pad".
- i. **Lot area, gross.** The total horizontal area included within the lot lines of a lot.
- J. **Lot area, net.** The area within the lot lines exclusive of any dedications for public rights-of-way, streets, parks, school sites, open space, easements, or other impediments which prevent the property owner from constructing a structure on that portion of the site.
- k. **Lot frontage.** The dimension of a lot or portion of a lot abutting on a street, except the side of a corner lot.
- l. **Lot width.** The average horizontal distance between side lot lines measured at right angles to the lot depth. In the case of lots at end of cul de sacs, lot width shall be measured at the front yard setback, between site lot lines.
- m. **Lot depth.** The average horizontal distance between the front and rear lot lines measured in the mean direction of the side lot lines.
- n. **Building Projections.** Garages, shading devices and other building projections will conform to regulations defined in the Zoning Code.

9. Drainage facilities shall comply with the requirements of the City of Fontana Master Plan of Drainage and/or approval of City of Fontana City Engineer. Drainage facilities outside the public right-of-way shall also be approved by Building and Safety.
10. Sewer treatment, collection, and facilities shall comply with the standards of the City of Fontana and Chino Basin Municipal Water District.
11. Water, electricity, gas, telephone and cable services shall comply with the requirements of the Fontana Water Company, Southern California Edison, Southern California Gas Company, Pacific Bell and cable companies, respectively.
13. The Project shall be designed to comply with the requirements of all responsible police and fire agencies.
14. The City Traffic Engineer will indicate pavement markings, signs and future studies as needed. The precise alignment of Highland Avenue will be determined by future studies and will be subject to further environmental analysis and studies, including the possible preparation of a supplemental and additional environmental impact report. The project will be developed in compliance with all environmental requirements including, without limitation, compliance with required mitigation measures.
15. Minor modifications to the Specific Plan: the following items are considered minor modifications, do not require a specific plan amendment and are subject to the review and approval of the Community Development Director:
 - a. Utility alignment and minor utility phasing changes,
 - b. Arterial road alignment revisions when the center line moves less than 200 feet.
 - c. Decreases in project density,
 - d. Adjustments of planning area boundaries of less than 100 feet.

TABLE 6.1
RESIDENTIAL DEVELOPMENT STANDARDS

LAND USE DEVELOPMENT STANDARDS¹	SINGLE FAMILY DETACHED UNITS		
	7200 SQ. FT. LOTS	6200 SQ. FT. LOTS	5000 SQ. FT. LOTS
1. Minimum Net Lot Area²	7200 sq. ft.	6200 sq. ft.	5000 sq. ft.
2. Minimum Building Pad	5500 sq. ft.	5000 sq. ft.	3500 sq. ft.
3. Maximum Coverage	50% of site	50% of site	60% of site
4. Minimum Lot Width³	55 ft./60 ft.⁴	50 ft./55 ft.⁴	40 ft./45 ft.⁴
5. Minimum Lot Width/Side Street	10 feet	10 feet	10 feet
6. Minimum Lot Depth	90 feet	100 feet	50 feet
7. Building Height	2-1/2 stories	2 1/2 stories	2 1/2 stories
8. Minimum Front Building Setback	20 feet	20 feet	20 feet
9. Minimum Rear Building Setback	15 feet	15 feet	15 feet
10. Minimum Side Building Setback	5 ft./10 ft.⁵ 3 feet for accessory accessory building	5 ft./10 ft.⁵ 3 feet for accessory building	5 ft./10 ft.⁵ 3 feet for building
11. Front-on Garage Setback	20 feet	20 feet	20 feet
12. Side-on Garage Setback	10 feet	10 feet	10 feet
13. Minimum Parking Dimensions	N/A	N/A	N/A
14. Minimum Two Car Garage	20 ft. x 20 ft.	20 ft. x 20 ft.	20 ft. x 20 ft.
15. Minimum Private Yard	N/A	N/A	N/A
16. Required Common Useable Open Space	N/A	N/A	N/A
17. Minimum Off-Street Parking Spaces	2 covered/unit	2 covered/unit	2 covered/unit
18. Minimum Dwelling Size	1,400 sq. ft.	1,350 sq. ft.	1,250 sq. ft.

GENERAL NOTES

1. All setbacks are measured from the property line.
2. Minimum Net Lot Area may include retaining walls or slopes (2:1 max.) in side yard areas of no more than 3 feet in height or rear yard areas of no more than 6 feet in height.
3. Minimum lot width will be measured at the front building setback line.
4. The larger number applies to minimum corner lot/side street frontage situations. Cul de sac lots require 40' width measured at front building setback line.
5. A minimum five foot setback may be used. Ten feet for corner lot conditions.
6. The 10000 sq. ft. lot development standards will be per City Zoning Ordinance.

TABLE 6.1a
RESIDENTIAL DEVELOPMENT STANDARDS

LAND USE DEVELOPMENT STANDARDS	ATTACHED & DETACHED CLUSTER UNITS	
	ATTACHED CLUSTER	DETACHED CLUSTER
1. Building Height	35 ft.	35 ft.
2. Minimum Building to Sidewalk Setback	8 ft.	5 ft.
3. Minimum Building to Curb Face Setback	13 ft.	10 ft. ¹
4. Minimum Building to Building Separation	5 ft./20 ft. ²	8 ft. / 10 ft. ³
5. Minimum Building to Project Boundary Setback	10 ft.	10 ft.
6. Front-on Garage Setback	N/A	3 ft. ⁴
7. Minimum Parking Dimensions	N/A	N/A
8. Minimum Two Car Garage	20 ft. x 20 ft.	20 ft. x 20 ft.
9. Minimum Useable Private Yard	N/A	96 sq. ft. ⁵
10. Required Public Open Space	275 sf/unit	275 sf/ unit
11. Required Private Open Space	75 sf/ unit	150 sf/ unit
12. Minimum Off-Street Parking Spaces	2.0/unit (2-bd unit) 2.5/unit (3- bd unit) (Plus 1 per 3 units for guest) min. 2 enclosed garages per unit	(Plus 1 per 3 units for guest) min. 2 enclosed garages per unit
13. Minimum Dwelling Size	1,300 sq. ft.	1,600 sq. ft.

GENERAL NOTES

1. Recreational amenity building setback may be reduced to 5 ft. when adjacent to a private motorcourt drive or aisle.
2. 5 ft. minimum building separation. Building separations less than 20 ft. shall comply with California Building Code requirements for fire resistant construction.
3. 8 ft. minimum separation required for building side to side separations. 10 ft. minimum required for rear to rear building separations.
4. Minimum garage setback shall be 3 ft. or 18 ft. and be constructed of decorative paving.
5. Minimum dimensions for useable space shall be 12 ft. in at least one direction with no dimension less than 8 ft.
6. A minimum 12'x12' of useable private yard space

6.3.2 Residential Standards

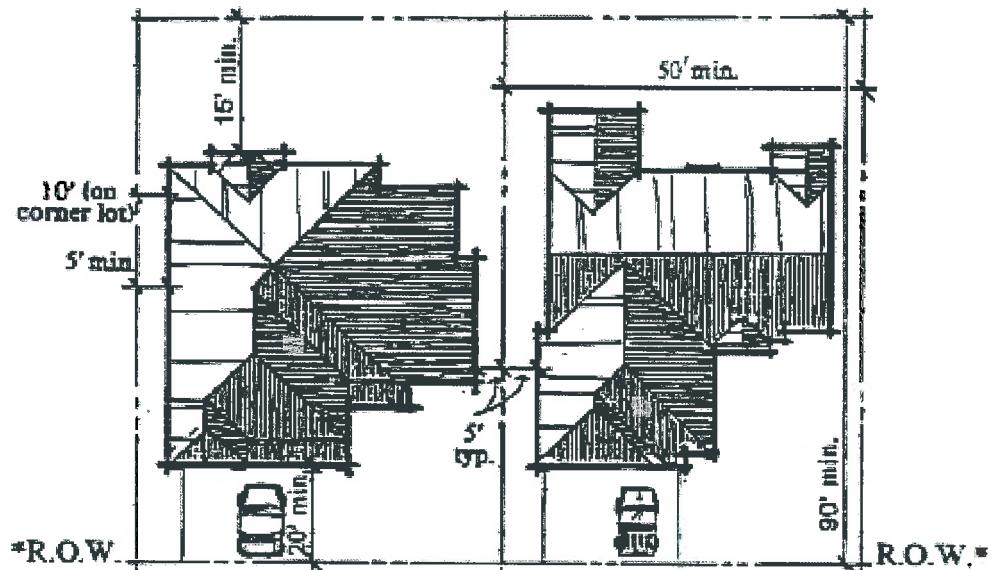
6.3.2.1 Single Family Detached (R-1-7200)

The standards of the R-1-7200 Zone shall include the following:

- A. Permitted Use - The following uses shall be permitted:
 1. Permanent Single-family detached dwelling unit on a permanent site with no more than one such dwelling.
 2. Private garages, the capacity of which shall not exceed three automobiles.
 3. Accessory buildings, structures and uses related to a permitted use.
 4. Service and utility facilities as required to implement the Plan.
 5. Public schools and parks
- B. Uses Not Permitted - Boarding and raising of livestock or farm animals including horses.
- C. Minimum Net Lot Area - The minimum net lot area shall be seven thousand two hundred (7,200) square feet.
- D. Maximum Site Coverage - The maximum building site coverage shall be fifty (50%) percent.
- E. Minimum Lot Width and Frontage - All lots shall have a minimum width of not less than fifty-five (55) feet. Street corner lots shall have a minimum width of sixty (60) feet. An exception will be on cul-de-sac conditions where the front lot width shall be forty (40) feet minimum measured at the front setback line.
- F. Building Height - Maximum height for all buildings shall be two and one-half (2-1/2) stories not to exceed thirty-five (35) feet.
- G. Minimum Front Building Setback - Front building setback shall be twenty (20) feet.
- H. Minimum Rear Building Setback - Rear building setback shall be a minimum

of fifteen (15) feet with a maximum encroachment of ten (10) feet for one story building projections.

- I. Minimum Side Building Setback - Minimum side yard setback shall be five (5) feet, with a minimum aggregate building separation between lots of ten (10) feet. A ten-foot side yard minimum shall be used for a corner lot street edge.
- J. Front-on Garages - The minimum set-back to any front-on garage shall be not less than twenty (20) feet. Measured same as minimum dimensions for guest parking spaces below.
- K. Side-on Garages - The minimum setback to any side-on garage shall be ten (10) feet.
- L. Minimum Dimensions of Guest Parking Spaces - Open guest parking spaces shall be permitted in a driveway having minimum dimensions of nine (9) feet wide and nineteen (19) feet long measured from the garage door to the nearest edge of the pedestrian walkway, property line, whichever is the lesser distance.
- M. Minimum Dwelling Size - The minimum dwelling size for a single family detached dwelling on a 7,200 square foot lot shall be 1,400 square feet. This area does not include garages or porches.
- N. Accessory Buildings - Accessory buildings and structures shall conform to City of Fontana Zoning Code.



TYPICAL PLOTTING

MINIMUM LOT SIZE	6,200 SQ.FT.
MAXIMUM COVERAGE	50%
MINIMUM PARKING SPACES	2 VEHICLE GARAGE PER UNIT

SINGLE FAMILY LOTS
6,200 SQ. FT.



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6-22

Exhibit 55

California
LANDINGS

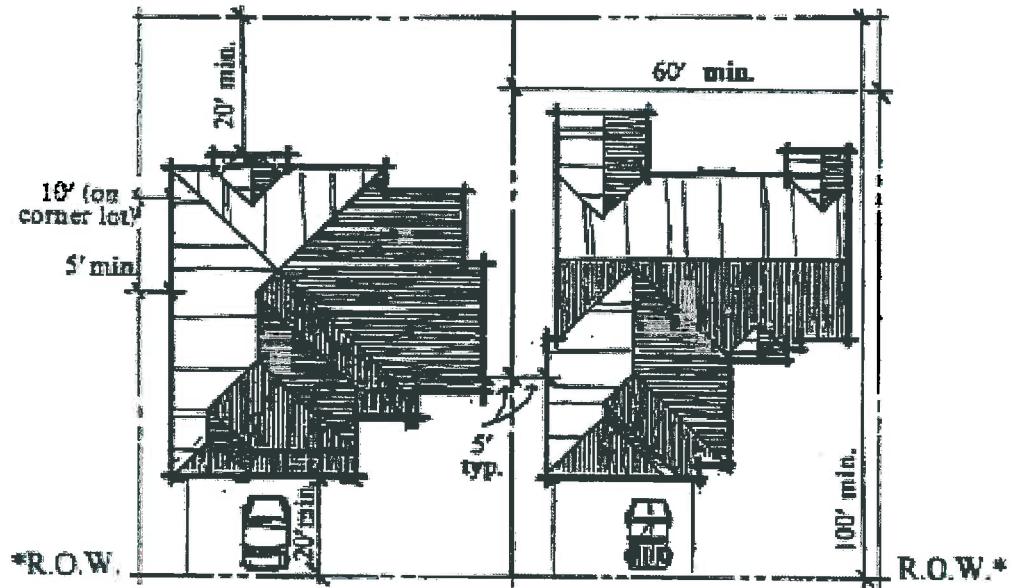
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6.3.2.2 Single Family Detached (R-1-6200)

The standards of the R-1-6200 Zone shall include the following:

- A. Permitted Uses:** The following uses shall be permitted in the R-1-6200 Zone:
 - 1. Permanent single-family detached dwelling unit on a permanent site with no more than one such dwelling.
 - 2. Private garages, the capacity of which shall not exceed three automobiles.
 - 3. Accessory buildings, structures and uses related to a permitted use.
 - 4. Service and utility facilities as required to implement the Plan.
 - 5. Public schools and parks
- B. Minimum Net Lot Area** - The minimum net lot area shall be six thousand two hundred (6,200) square feet.
- C. Maximum Site Coverage** - The maximum building site coverage shall be fifty (50%) percent.
- D. Minimum Lot Width and Frontage** - All lots shall have a minimum width of not less than fifty (50) feet. Street corner lots shall have a minimum width of fifty-five (55) feet. An exception will be on cul-de-sac conditions where the front lot width shall be forty (40) feet minimum measured at the front setback line.
- E. Building Height** - Maximum height for all buildings shall be two and one-half (2-1/2) stories not to exceed thirty-five (35) feet.
- F. Minimum Front Building Setback** - Front building setback shall be twenty (20) feet.
- G. Minimum Rear Building Setback** - Rear building setback shall be a minimum of fifteen (15) feet with a maximum encroachment of ten (10) feet for one story building projections.

- H. Minimum Side Building Setback - Minimum side yard setback shall be five (5) feet, with a minimum aggregate building separation between lots of ten (10) feet. A ten-foot side yard minimum shall be used for a corner lot street edge.
- I. Front-on Garages - The minimum set back to any front-on garage shall be not less than twenty (20) feet. Measured same as minimum dimensions for guest parking spaces below.
- J. Side-on Garages - The minimum setback to any side-on garage shall be ten (10) feet.
- K. Minimum Dimensions of Guest Parking Spaces - Open guest parking spaces shall be permitted in a driveway having minimum dimensions of nine (9) feet wide and nineteen (19) feet long measured from the garage door to the nearest edge of the pedestrian walkway, property line, whichever is the lesser distance.
- L. Minimum Dwelling Size - The minimum dwelling size for a single family detached dwelling on a 6,200 square foot lot shall be 1,350 square feet. This area does not include garages or porches.
- M. Accessory Buildings - Accessory buildings and structures shall conform to City of Fontana Zoning Code.



TYPICAL PLOTTING

MINIMUM LOT SIZE	7200 SQ.FT.
MAXIMUM COVERAGE	50%
MINIMUM PARKING SPACES	2 VEHICLE GARAGE PER UNIT

*Note: On Local Streets, 20' minimum front yard setback measured from back of P.U.E.

SINGLE FAMILY LOTS
7,200 SQ. FT.

Exhibit 56



2004

California
LANDINGS

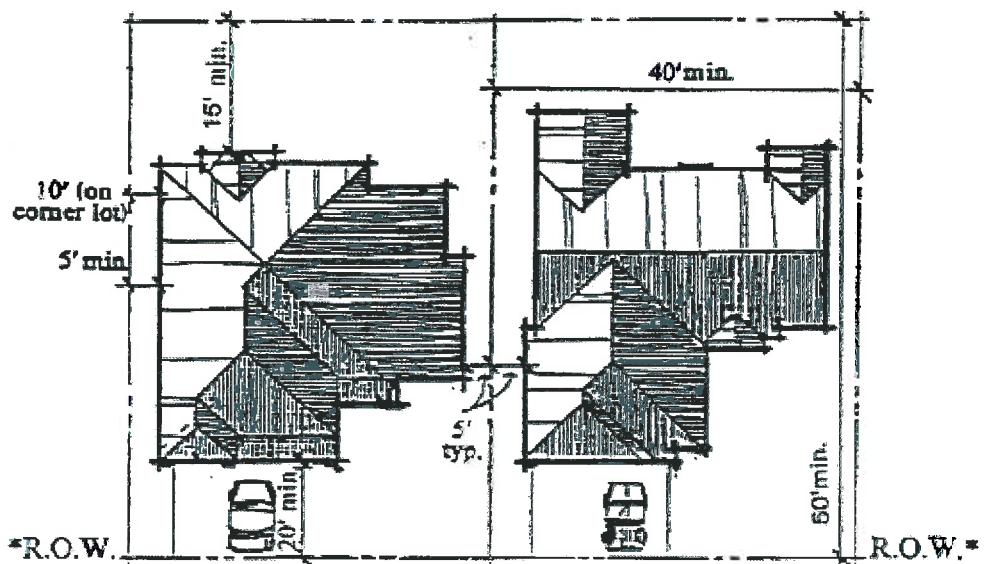
Kaufman & Broad ▲

6.3.2.3 Single Family Detached (R-2-5000)

The standards of the R-2-5000 Zone shall include the following:

1. **Permitted Use** - The following uses shall be permitted in the R-2-5000 Zone:
 1. Permanent Single-family detached dwelling unit on a permanent site with no more than one such dwelling.
 2. Private garages, the capacity of which shall not exceed three automobiles.
 3. Accessory buildings, structures and uses related to a permitted use.
 4. Service and utility facilities as required to implement the Plan.
 5. Public schools and parks
- B. **Minimum Net Lot Area** - The minimum net lot area shall be five thousand (5,000) square feet.
- C. **Maximum Site Coverage** - The maximum building site coverage shall be sixty (60%) percent.
- D. **Minimum Lot Width and Frontage** - All lots shall have a minimum width of not less than forty (40) feet. Street corner lots shall have a minimum width of fifty (50) feet. An exception will be on cul-de-sac conditions where the front lot width shall be forty (40) feet minimum measured at the front setback line.
- E. **Building Height** - Maximum height for all buildings shall be two and one-half (2-1/2) stories not to exceed thirty-five (35) feet.
- F. **Minimum Front Building Setback** - Front building setback shall be twenty (20) feet.
- G. **Minimum Rear Building Setback** - Rear building setback shall be a minimum of fifteen (15) feet with a maximum encroachment of ten (10) feet for one story building projections.

- H. Minimum Side Building Setback - Minimum side yard setback shall be five (5) feet, with a minimum aggregate building separation between lots of ten (10) feet. A ten-foot side yard minimum shall be used for a corner lot street edge.
- I. Front-on Garages - The minimum set back to any front-on garage shall be not less than twenty (20) feet. Measured same as minimum dimensions for guest parking spaces below.
- J. Side-on Garages - The minimum setback to any side-on garage shall be ten (10) feet.
- K. Minimum Dimensions of Guest Parking - Open guest parking spaces shall be permitted in a driveway having minimum dimensions of nine (9) feet wide and nineteen (19) feet long measured from the garage door to the nearest edge of the pedestrian walkway, property line, whichever is the lesser distance.
- L. Minimum Dwelling Size - The minimum dwelling size for a single family detached dwelling on a 5,000 square foot lot shall be 1,250 square feet. This area does not include garages or porches.
- M. Accessory Buildings - Accessory buildings and structures shall conform to City of Fontana Zoning Code.



TYPICAL PLOTTING

MINIMUM LOT SIZE	5,000 SQ.FT.
MAXIMUM COVERAGE	60%
MINIMUM PARKING SPACES	2 VEHICLE GARAGE PER UNIT

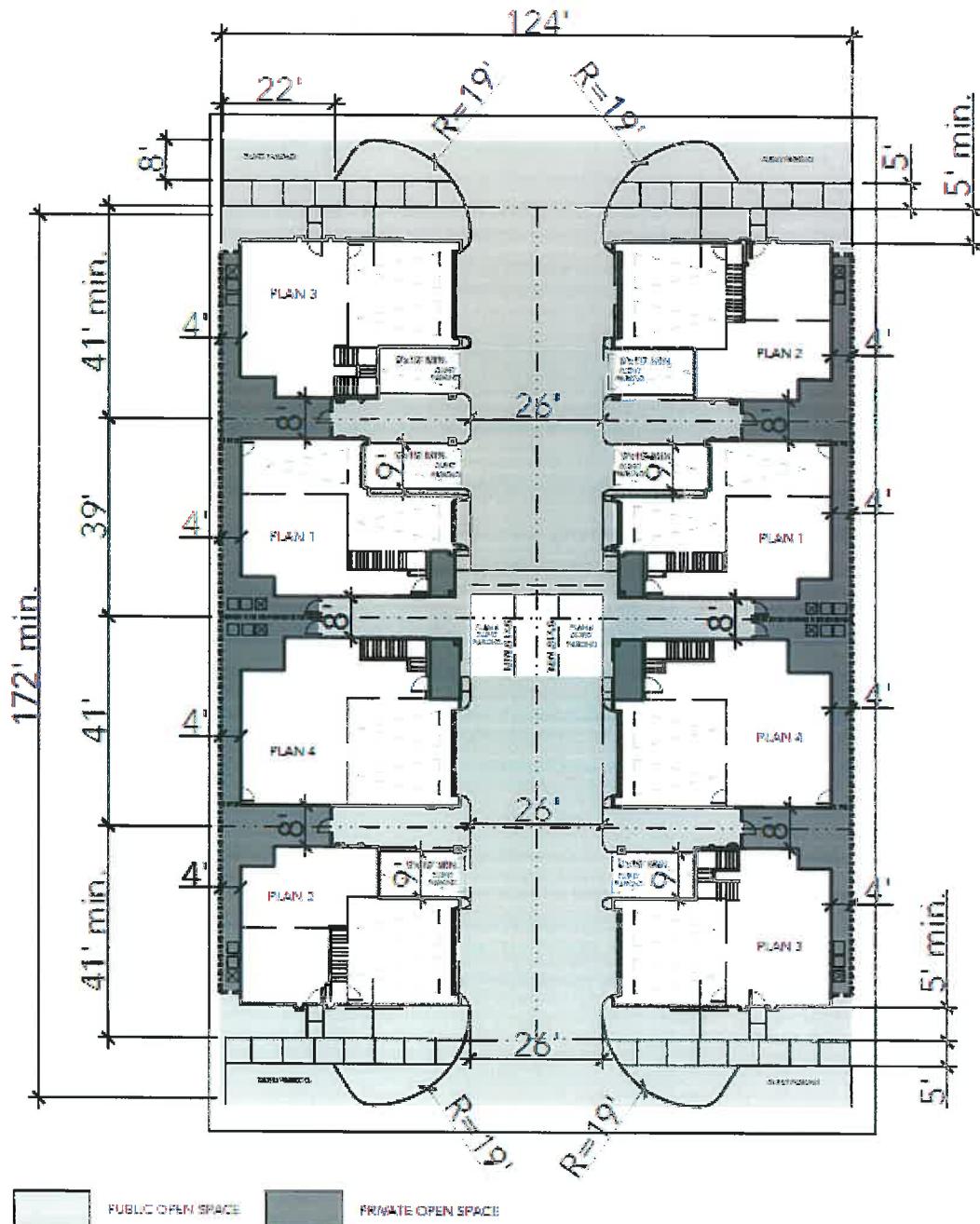
SINGLE FAMILY LOTS
5,000 SQ. FT.

Exhibit 57



LANDINGS

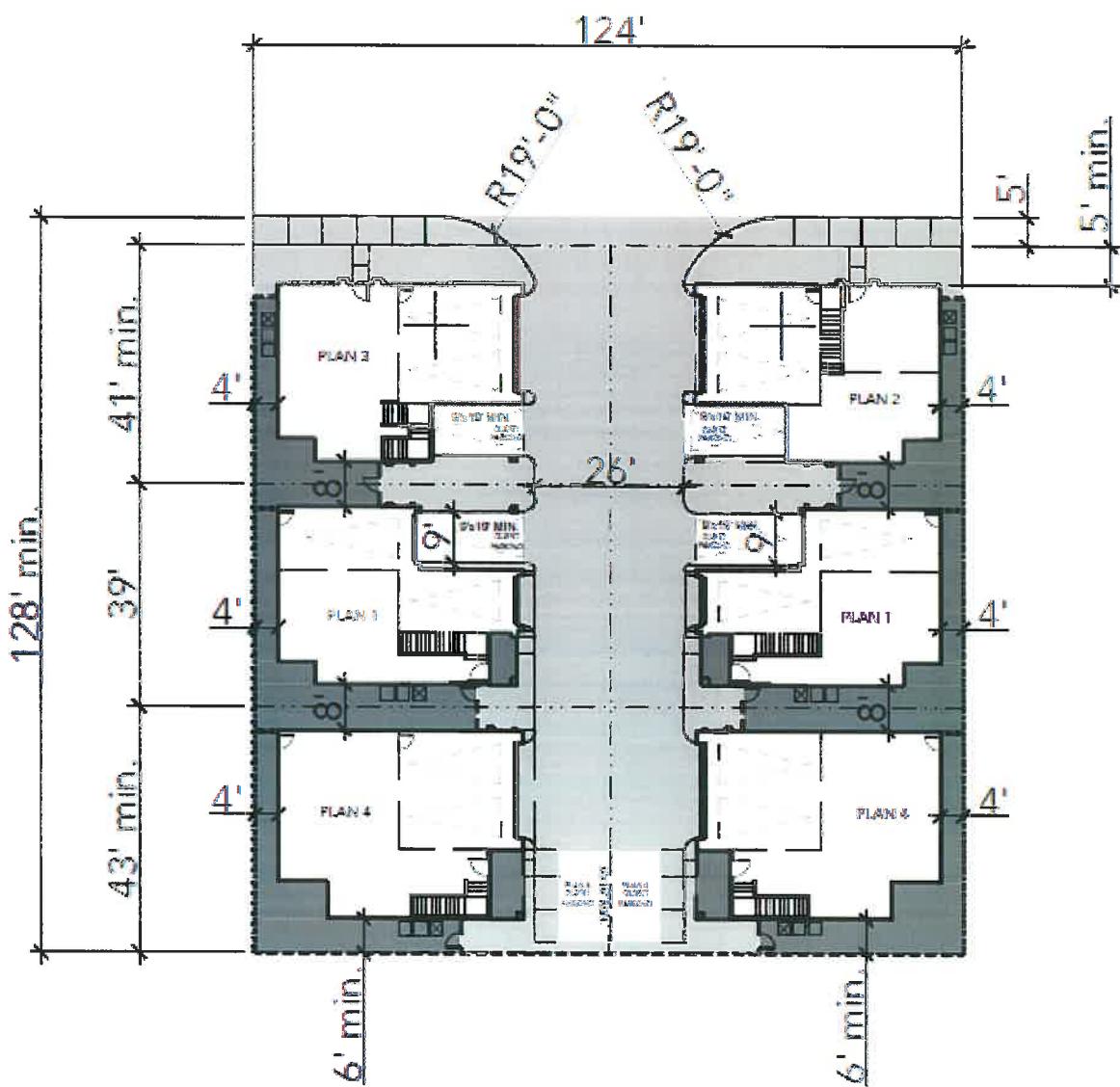
California
LANDINGS



**TYPICAL
DETACHED
CLUSTER - 8 PACK**

Exhibit 58

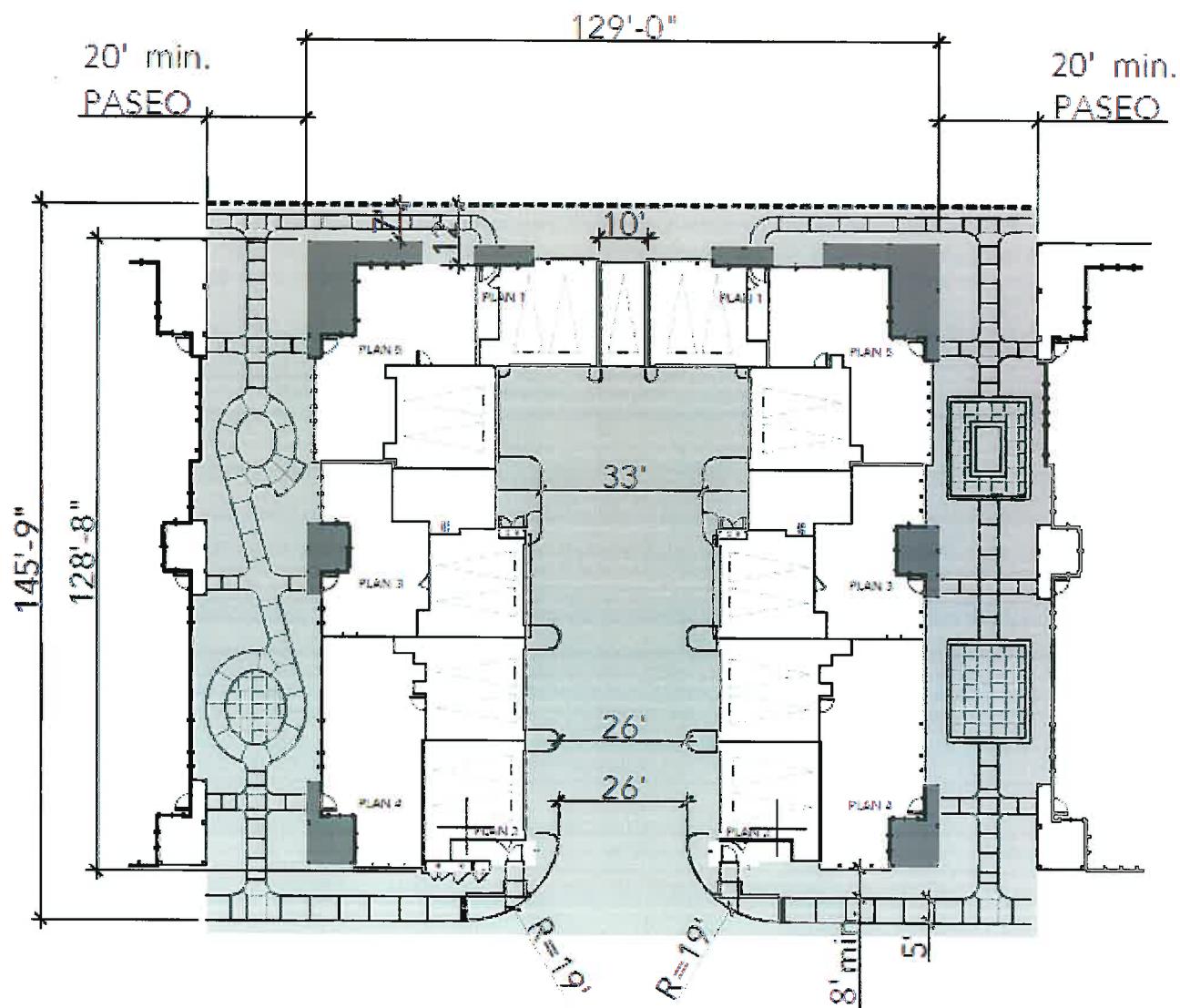
California
LANDINGS



TYPICAL DETACHED CLUSTER - 6 PACK

Exhibit 59

California
LANDINGS



**TYPICAL
ATTACHED
CLUSTER**

Exhibit 60

California
LANDINGS

6.3.3 Detached & Attached Cluster Residential

The standards of the Detached & Attached Cluster Residential area shall include the following:

1a. **Permitted Uses** - The following uses are permitted within the Detached & Attached Cluster Residential areas:

- a. Single-family detached dwellings
- b. Single-family attached dwellings
- c. Multiple-family dwellings (for sale units only)
- d. Public park or playgrounds

1b. **Conditionally Permitted Uses**

- a. Planned Unit Developments
- b. Senior Housing
- c. Assisted Living
- d. Day Care
- e. Fire Stations
- f. Residential Care Facilities

2. Maximum Structural Height – Maximum height of all buildings and structures shall be two and one-half (2-1/2) stories, not to exceed thirty-five (35) feet.

3. Building Setbacks – See Table 6.1.a

4. Building Separations – See Table 6.1.a

6.3.4 Sign Regulations

The purpose of this section is to establish standards for the uniformity and hierarchy of signage, reinforcing the character and image of the Planned Community. The following regulations shall apply to all residential and nonresidential uses within the California Landings Specific Plan.

Except as specified below, sign regulations shall be in accordance with Chapter 3 of the Fontana Municipal Code and all standards set forth under the controlling zone district in which the signs are located. Signs shall be established as part of the Sign Program administered by the City and shall conform to the following general specifications.

Community Entry Monumentation

The following sets forth standards for the implementation of the Community Entry Monumentation concept for the California Landings community. Entry monumentation will be provided at levels consistent with the provisions below:

A. Primary Entry Monumentation

Large scale monumentation signage identifying major entries to the California Landings Community.

1. Maximum Height and Area: Such signage shall be provided as an element of the required community theme wall; such wall shall not exceed a vertical height of six (6) feet, nor a total area of two hundred fifty (250) square feet.
2. Maximum Sign Area: Forty (40) square feet of sign area to include community name and/or logo.

B. Secondary Entry Monumentation

Medium scale monumentation identifying secondary entries to the California Landings Community (see Exhibit 37).

1. Maximum Height and Area: Such signage will be part of a planter wall and shall not exceed a vertical height of four (4) feet, nor a total area of one hundred sixty (160) square feet.
2. Maximum Sign Area: Thirty-five (35) square feet of sign area to include community name and/or logo.

C. Neighborhood Entry Signs

Low scale monumentation signage providing identification and entry into the residential neighborhood. (See Exhibit 38).

1. Maximum Height and Area: If provided as an element of the required community theme wall, such signage shall not exceed a vertical height of four (4) feet, nor a total area of one hundred-forty five (145) square feet. If such signage is provided as free standing, such sign shall not exceed a vertical height of three (3) feet, nor a total area of not more than seventy-five (75) square feet.
2. Maximum Sign Area: Twenty-five (25) square feet of sign area to include project name and/or logo.

D. Product Entry Signs

Small scale monumentation signage providing identification of entry into an individual project area.

1. Maximum Height and Area: Such signage shall be provided as an element of a wall, such signage shall not exceed a vertical heights of six (6) feet, nor a total area of eighty-four (84) square feet.
2. Maximum Sign Area; Twenty-five (25) square feet of sign area to include project name and/or logo.

E. Future Development Signs

Temporary signs advising of future development using the general terms of residential or commercial in the description of the future land use on the site upon which the sign shall be located.

1. Such signs shall not exceed a vertical height of twelve (12) feet, a horizontal length of ten (10) feet, nor a total area of fifty (50) square feet.
2. Signs shall be consistent in size, color, material and design as approved by the Community Development Director, and shall provide an identifiable element to the community.

F. Temporary Signs

Temporary signs denoting the builder/architect, engineer or contractor placed on the premises where construction is in progress.

1. Such signs shall not exceed a vertical length of eight (8) feet, nor a total area of thirty-two (32) square feet.

G. Off-Site Signs

Any off-site signage for proposed development will conform to Building Industry Association (BIA) sign program.

6.4 PLAN ADMINISTRATION

6.4.1 Development Project Review Process

1. Purpose and Intent

This section references procedural and content requirements for the review and approval of development projects in planning unit areas within the adopted specific plan. It is the intent of this section to establish applicability of appropriate sections within Chapter 33 of the City's ordinances which set forth clearly defined procedures for the streamlined review of such development while insuring consistent implementation of the objectives, standards and quality features of each specific plan community.

2. Applicability

Procedures referred to herein shall apply to all planning unit areas within the California Landings Specific Plan area.

3. Procedural Overview

The topics covered in the City's Development Code which apply to administration of this Specific Plan include:

a) Processing

- 1) Pre-filing conference (if required) with Planning Staff
- 2) Application Fees
- 3) Application Requirements
- 4) Environmental Review/ Clearance
- 5) Development Advisory Board (DAB) Review
- 6) Planning Commission Review, if necessary by State Law
- 7) City Council Review, if necessary State Law
- 8) Permit Expiration

b) General Administration

- 1) Planning Unit Area or Project Review
- 2) Confirmatory
- 3) Capital Improvements Funding
- 4) Zoning/Land Use Districts
- 5) Land Use District Boundaries
- 6) Special Districts

- 7) Determination of Uses
- 8) Conditional Uses
- 9) Variances and Adjustment
- 10) Certificates of Occupancy
- 11) Development Agreement
- 12) Enforcement
- 13) Amendment/Modifications
 - a. Amendment procedures
 - b. Notices
 - c. Findings and conditions, use of
 - d. Effective date

6.4.2 Conditional Uses

- 1. Reference: Conditional Use Permits, City Development Code

6.4.3 Variances

- 1. Reference: Variances, City Development Code

6.4.4 Specific Plan Amendments

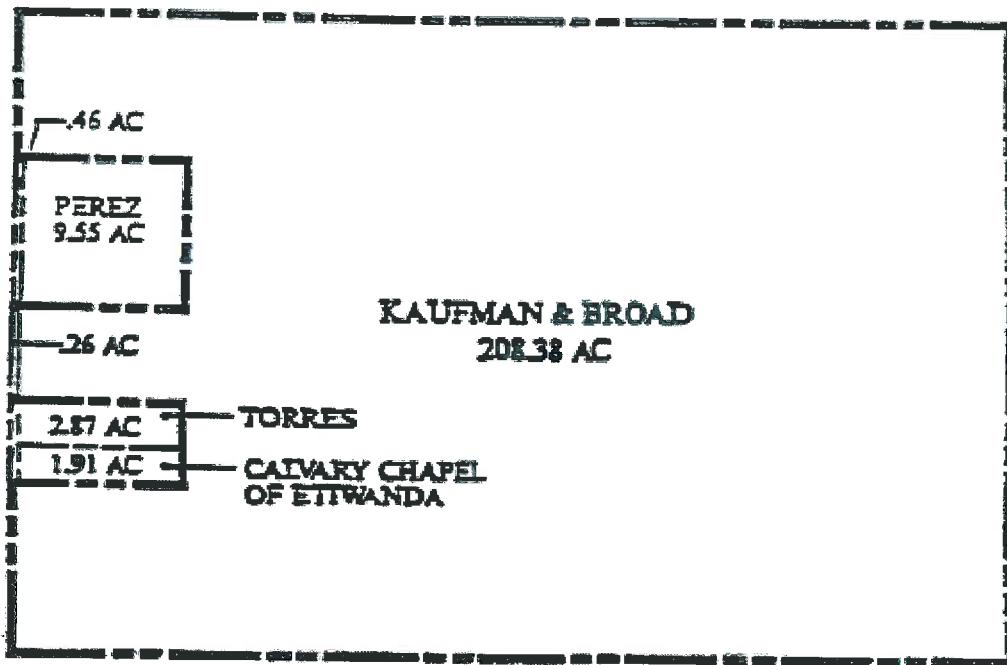
- 1. California Governmental Code for Specific Plans Sections 65450-65457
- 2. Section 65453: Specific plans shall be prepared, adopted, and amended in the same manner as general plans, except that a specific plan may be adopted by resolution or ordinance.
- 3. Minor amendments may be approved by City Staff (see General Notes). Amendments deemed significant must be approved by the City of Fontana Planning Commission and City Council (see Chapter 33, Article XXXI, Sec. 33-176(b), City Development Code).

7.0 APPENDIX

7.1 PROPERTY OWNERSHIP

- Kaufman and Broad owns 208.3 acres of the Specific Plan area
Assessor parcels 228-031-03; 228-041-03; 228-021-16,18,22-24.
- The remaining 14.33 acres of other ownership parcels are owned by:
 - * Conrado and Juana Perez: 9.55 acres, assessor parcel 228-021-17
 - * Ramiro and Fermina Torres: 2.87 acres, assessor parcel 228-021-15
 - * Calvary Chapel of Etiwanda: 1.91 acres, assessor parcel 228-021-14

Exhibit 61



Note: Project acreage from Assessor's Office and Project Engineer take-offs.

72 GENERAL PLAN POLICY CONSISTENCY

(General Plan policies are paraphrased, see General Plan for full policy.)

LAND USE ELEMENT

3.1.1 Policy: Establish strong economic base

Consistency: California Landings will establish a strong economic base by adding up to 15 acres of commercial tax revenue to the city's tax base. A Fiscal Impact Analysis provided in the EIR will document the revenues provided by this project.

3.1.2 Policy: Encourage residential variety

Consistency: The Specific Plan delineates a variety of residential densities. Single family detached residential land uses are proposed on site including 7,200, 6,200 and 5,000 square foot lots.

3.1.3 Policy: Support "upscale" housing

Consistency: "Upscale" single family housing will be supported by the California Landings Specific Plan, which proposes that all of its housing as single family detached lots.

3.1.4 Policy: Encourage office/commercial

Consistency: California Landings conforms with this policy by proposing community mixed use (office/commercial) along Route 30 and Highland Avenue, as delineated on the Land Use Policy Map.

3.2.1 Policy: Maximize land use quality

Consistency: The land uses proposed in the Specific Plan conform to the land use policy map. Residential neighborhoods will be buffered and separated from commercial uses by Highland Avenue. Other adjacent land uses are compatible and do not require buffering.

3.2.2 Policy: Facilitate regional uses

Consistency: The Specific Plan will integrate proposed transportation corridors (such as State Route 30) and other regionally beneficial land uses, as applicable.

3.2.3 Policy: Encourage infill

Consistency: The California Landings Specific Plan will encourage infill by including the three outparcels into the Specific Plan area and assuring that they have access in order to provide compatible contiguous land uses.

3.3.1 Policy: Promote commercial centers, discourage strip development

Consistency: Commercial/office development within the site is proposed along Highland Avenue and State Route 30, as indicated on the Land Use Policy Map. A potential H.O.V. interchange at Route 30 and Beech Avenue establishes this commercial area of the Specific Plan adjacent to a significant highway intersection.

3.3.2 Policy: Encourage incremental development

Consistency: The Specific Plan will conform to this policy by proposing development along the proposed expansion of an existing highway (Route 30) and by phasing the project incrementally to respond to market conditions.

3.3.3 Policy: Manage growth in relation to public facilities

Consistency: Proposed development will be implemented so as not to exceed the ability of the City, special districts and/or utilities to provide needed facilities and services.

3.3.4 Policy: Require new development to pay facility and service costs

Consistency: The Specific Plan project proponent will pay required fair share costs of public facilities and services needed to serve the Specific Plan site.

3.3.5 Policy: Pursue responsible annexations

Consistency: NA: No annexations are required or pursued here as the site is within City limits.

3.3.6 Policy: "Master Capital Facilities/Services Study and Plan" required for specific plan or subdivision map.

Consistency: Before the tentative map is approved, the Master Capital Facility Plan will be approved, or a development agreement executed for required infrastructure, or a general plan amendment will be approved.

3.3.7 Policy: One dwelling unit/structure per existing lot of record.

Consistency: Necessary infrastructure and services will be provided for the Specific Plan area in order to conform to this policy.

CIRCULATION ELEMENT

4.1.1 Policy: Provide street classifications

Consistency: Various street classifications proposed are described in the Specific Plan, all designed to serve the traffic needs of the area. The proposed public road classifications range in size from primary highway to local collector street.

4.1.2 Policy: Limit access to arterials.

Consistency: To maintain desired quality of traffic flow, direct access to arterials will be limited within the Specific Plan area.

4.1.3 Policy: Design for anticipated need.

Consistency: Streets proposed in the Specific Plan have been reviewed and approved by the City Planning and Transportation Departments to provide adequate service. A traffic analysis will be undertaken in the project EIR to confirm traffic capacities.

4.1.4 Policy: Regulate intensity of use re: traffic.

Consistency: Land uses proposed in the Specific Plan are designed to conform to this policy of regulating balance of traffic and road capacity.

4.1.5 Policy: Design traffic control to meet City needs.

Consistency: Traffic control measures proposed in the Specific Plan and EIR will meet City standards to ensure that roads function as intended.

4.1.6 Policy: Meet safety standards.

Consistency: The proposed Specific Plan will adhere to uniform practices for safety.

4.1.7 Policy: Coordinate Improvements.

Consistency: Proposed street improvements and signalization will coordinate with regional transportation goals.

4.1.8 Policy: Maintain LOS C.

Consistency: Wherever possible, existing and proposed streets will be improved to maintain a LOS "C". A traffic analysis will be undertaken in the project EIR to analyze levels of service.

4.1.9 Policy: Locate major traffic generators to protect local streets.

Consistency: In conformance with this policy, circulation proposed will not encourage the use of local residential streets for access.

4.1.10 Policy: Provide bus turnouts.

Consistency: Bus turnouts will be provided along appropriate arterials, where required.

4.1.11 Policy: Plan interchange I-10 at Alder and Beech.

Consistency: NA: This interchange is beyond the scope of this Specific Plan.

4.1.12 Policy: Use left-turn pockets.

Consistency: Left turn pockets will be used where feasible at major intersections.

4.2.1 Policy: Regional transportation

Consistency: Regional transportation will be facilitated by the future Route 30 Freeway with a possible high occupancy on-off ramp at Beech.

4.2.2 Policy: Support regional bus systems

Consistency: The proposed Specific Plan will support regional bus service by providing a bus turnout along an appropriate route.

4.2.3 Policy: Provide a "Trans Center"

Consistency: NA: A plan for a "Trans Center" is beyond the scope of this Specific Plan.

4.3.1 Policy: Coordinate with SCAG, SBCTC, SCRTD

Consistency: The proposed Specific Plan will cooperate with the City policies in coordinating with the various transit planning efforts.

4.3.2 Policy: Recognize private transport services

Consistency: Future residents of the Specific Plan may utilize private transportation services.

4.3.3 Policy: Coordinate with human services

Consistency: The Specific Plan will encourage (within its scope) coordination between agencies to reduce duplicate transit services.

4.3.4 Policy: Require new development to put in bus shelters

Consistency: California Landings will provide transit facilities (shelters and turn-outs) where appropriate, as required.

4.3.5 Policy: Provide for disabled persons

Consistency: Public transportation facilities provided within the Specific Plan property will be designed to ensure accessibility of disabled persons.

4.3.6 Policy: Regional transportation

Consistency: Regional transportation will be facilitated by the future Route 30 Freeway with a possible high occupancy on-off ramp at Beech.

4.3.7 Policy: Support regional bus systems

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Consistency: California Landings will provide transit facilities (shelters and turn-outs) where appropriate, as required.

4.4.6 Policy: Provide for disabled persons

Consistency: Public transportation facilities provided within the Specific Plan property will be designed to ensure accessibility of disabled persons.

4.4.7 Policy: Use right-of-way for bikes

Consistency: As described above, the street R.O.W.s will be utilized to contain bike lanes and/or trails as described in the Specific Plan. An off-road bike path will be provided in the north side of Walnut Avenue Parkway as requested by the City.

4.4.8 Policy: Encourage pedestrian trails in hills

Consistency: NA: The site is not within a hilly area.

4.4.9 Policy: Maintain pedestrian trails

Consistency: Pedestrian walks will be maintained with the public streets they follow, in conformance with City policy.

4.5.1 Policy: Designate truck routes

Consistency: NA: It is beyond the scope of this Specific Plan to designate truck routes. Highland Avenue is designated as a truck route in the City Circulation Element.

4.5.2 Policy: Design truck routes for safety

Consistency: Roadways that are City designated truck routes will be designed to safely accommodate truck travel.

4.5.3 Policy: Develop buffering adjacent truck routes

Consistency: Appropriate barriers will be installed as required to minimize noise impacts to sensitive land uses.

4.5.4 Policy: Provide adequate loading/unloading areas

Consistency: Commercial areas within the Specific Plan will provide sufficient and safe loading areas.

4.5.5 Policy: Enforce truck regulations

Consistency: NA: Enforcement of truck regulations is beyond the scope of this Specific Plan.

4.6.1 Policy: Ensure safe rail operations

Consistency: NA: Safety of the rail system is beyond the scope of this project.

4.6.2 Policy: Establish connections between rail and activity centers

Consistency: NA: Inter-city railway is beyond the scope of this project.

4.6.3 Policy: Provide spur access to industrial areas

Consistency: NA: No industrial areas are proposed within the site or in the vicinity which would require spur access via railroad.

4.6.4 Policy: Develop safe rail crossings

Consistency: NA: No railway intersections exist or are proposed on site.

4.6.5 Policy: Provide appropriate noise attenuation

Consistency: If noise impacts to the Specific Plan area are indicated in the project EIR, appropriate mitigation measures will be undertaken.

4.6.6 Policy: Analyze potential for rail station

Consistency: NA: Viability of a rail station between Orange Way and Ceres Avenue is beyond the scope of this Specific Plan.

HOUSING

5.1.1 Policy: Provide a range of residential development densities (1 du/ac to 12 du/ac)

Consistency: The project proposes 5,000 square foot, 6,200 square foot and 7,200 square foot lots.

5.1.2 Policy: Provide for infill multi-family along commercial corridors.

Consistency: The specific plan does not propose the development of infill multi-family residential uses.

5.1.3 Policy: Allow second units on existing single family lots (Sec. 33-22)

Consistency: The specific plan will comply with the conditions established in Section 33-22 of the Development Code where applicable.

5.1.4 Policy: Encourage low income affordable housing

Consistency: 5,000 square foot lots will be developed within the proposed project to provide for a range of housing choices. See policy 5.15.

5.1.5 Policy: Respond to State requirements for low and moderate income housing through incentive techniques.

Consistency: 5,000 square foot lots will be developed within the proposed project to provide for a range of housing choices.

5.1.6 Policy: Require affordable units for which density bonuses or other incentives are granted to meet general plan criteria.

Consistency: 5,000 square foot lots will be developed within the proposed project to provide for a range of housing choices. See policy 5.15.

5.1.7 Policy: Require that low and moderate income households will not be located in a single area in Fontana.

Consistency: NA: This policy is beyond the scope of this specific plan.

5.1.8 Policy: Encourage elderly housing with 25% density bonus or other financial incentive.

Consistency: NA: Senior housing is not proposed within the project site.

5.1.9 Policy: Encourage handicap accessibility especially regarding apartment complexes.

Consistency: The specific plan will comply with state law and the city's codes, ordinances and standards in regards to handicap accessibility. However, the project does not propose to develop apartment complexes.

5.1.10 Policy: Encourage new housing for low income, elderly and disabled person to be close to public transportation.

Consistency: The specific plan will comply with the city's codes, ordinances and standards in regards to the provision of convenient access to public transportation.

5.1.11 Policy: Encourage child care development coincident with new housing.

Consistency: A child care center is a permitted use within the commercial mixed use designation of the Specific Plan.

5.1.12 Policy: Address homeless needs/ allow emergency or temporary shelters in Medium and Medium-High Density Residential areas (subject to C.U.P)

Consistency: The location of emergency or temporary shelter for the homeless would not be an appropriate facility within California Landings. Typically, such centers are located in downtown urban centers close to social services.

5.1.13 Policy: Allow priority review for low and moderate income and elderly housing.

Consistency: The project does not propose low and moderate income or elderly housing.

5.1.14 Policy: Encourage favorable home purchasing techniques.

Consistency: Kaufman & Broad Mortgage Company provides competitive and priority financing for home buyers in Kaufman & Broad projects.

5.1.15 Policy: Utilize 20% Redevelopment set-aside funds for multi-family mortgage revenue bond projects.

Consistency: Use of such set-aside funds will be considered.

5.1.16 Policy: Encourage energy conservation and passive design concepts.

Consistency: The Specific Plan will comply with the city's codes, ordinances and standards in regards to the implementation of energy conserving practices.

5.2.1 Policy: Prohibit discrimination

Consistency: Kaufman & Broad practices a non-discriminatory policy in the selection of prospective home buyers.

5.2.2 Policy: Support further spatial deconcentration and fair housing opportunities.

Consistency: California Landings supports the spatial deconcentration and fair housing opportunities as indicated in the General Plan land use plan.

5.2.3 Policy: Prohibit practices which restrict housing choice.

Consistency: Kaufman & Broad practices a non-discriminatory policy to offer the best possible housing selection for a prospective client regardless of race, sex, income, etc.

5.3.1 Policy: Advocate substandard property rehabilitation.

Consistency: NA: The Specific Plan proposes the development of a new community.

5.3.2 Policy: Continue existing rehabilitation programs.
Consistency: NA: The Specific Plan proposes the development of a new community. There is no existing rehabilitation program on the project site.

5.3.3 Policy: Focus rehabilitation efforts in select target areas.
Consistency: NA: The Specific Plan proposes the development of a new community.

5.3.4 Policy: Coordinate with County for rehabilitation in unincorporated areas.
Consistency: NA: The Specific Plan proposes the development of a new community. There is no existing housing on the project site to be reconstructed or rehabilitated.

5.3.5 Policy: Mitigate displacement impacts as a result of demolition.
Consistency: NA: There are no inhabited structures to be demolished at this time.

5.4.1 Policy: Retain existing economically and physically sound neighborhoods.
Consistency: NA: There are no residential neighborhoods on-site..

5.4.2 Policy: Retain existing viable mobile home parks.
Consistency: There are no existing mobile home parks.

5.4.3 Policy: Promote property maintenance.
Consistency: A landscape and lighting district is proposed to maintain the physical appearance.

5.4.4 Policy: Enforce building, safety and housing codes.
Consistency: NA: Enforcement of building safety and housing is a City function.

COMMUNITY DESIGN ELEMENT

6.1.1 Policy: Establish development standards

Consistency: The California Landings Specific Plan establishes development standards and proposes land uses compatible with the surrounding area and City goals.

6.1.2 Policy: Consider adjacent development

Consistency: The proposed Specific Plan is designed to consider opportunities and constraints of adjacent development, including the proposed improvements on State Route 30, Lytle Creek Specific Plan to the north, Rancho Fontana Specific Plan to the south and single family development to the east.

6.2.1 Policy: Ensure variety in large development

Consistency: The Specific Plan provides design standards establishing architectural themes and with variety for residential and commercial development.

6.2.2 Policy: Establish landscape requirements

Consistency: The Specific Plan provides landscape standards that provide adequate environmental controls and pleasing aesthetics. The specific plan shall comply with all city landscape standards in effect at the time of development.

6.2.3 Policy: Active code enforcement

Consistency: NA: Code enforcement is beyond the scope of the Specific Plan.

6.2.4 Policy: Landscape medians

Consistency: The Specific Plan proposed landscape medians within Beech Avenue and Highland Avenue. Landscape medians shall be designed as per city median standards and as per direction of

City Community Development Department / Engineering Division.

6.3.1 Policy: Encourage preservation

Consistency: NA: there are no structures or elements on-site that require preservation.

6.3.2 Policy: Provide incentives for preservation

Consistency: NA: incentives program for preservation and restoration is beyond the scope of this Specific Plan.

6.3.3 Policy: Ensure ordinance consistency with preservation

Consistency: NA: ordinance consistency is beyond the scope of this Specific Plan.

6.3.4 Policy: Establish historic district

Consistency: NA: establishment of downtown historic district is beyond the scope of this Specific Plan.

6.4.1 Policy: Protect natural character of hills

Consistency: NA: site does not contain any part of San Gabriel Mountains or Jurupa Hills to protect.

6.4.2 Policy: Establish open space network

Consistency: The project is linked by the street parkway pedestrian and bicycle trails to project parks, as well as to the area-wide open space.

6.4.3 Policy: Promote commercial core design theme

Consistency: NA: commercial core area of old Fontana and its revitalization is beyond the scope of this Specific Plan.

6.4.4 Policy: Establish arterial corridor design theme

Consistency: The Specific Plan describes individual design themes for neighborhoods and major roads within the Specific Plan area.

6.4.5 Policy: Establish Civic Center design theme

Consistency: NA: City hall/civic center design theme is beyond the scope of this Specific Plan, and no public facilities are proposed on-site at this time.

ECONOMIC DEVELOPMENT ELEMENT

7.2.1 Policy: Encourage full range of -commercial uses

Consistency: The California Landings proposes approximately 22 acres (10% of site) of commercial/retail uses to serve residents of the site, provide employment and contribute to the local tax base.

7.2.2 Policy: Limit adverse effects on existing business.

Consistency: The commercial development proposed at California Landings will not create adverse impacts on the Central Business District.

7.2.3 Policy: Cluster commercial uses.

Consistency: It is the intent of the commercial development to provide a variety of shopping and service needs of residents conveniently, to enable one-stop shopping.

7.2.4 Policy: Promote industrial development.

Consistency: The commercial development proposed will provide employment for resident labor force in conformance with this policy.

7.2.5 Policy: Arrest blight in retail areas.

Consistency: The Specific Plan proposes design goals and a maintenance program to prevent future deterioration of commercial/retail areas.

7.2.6 Policy: Monitor fiscal impact of new development.

Consistency: A fiscal impact report will be prepared as part of the project EIR.

7.2.7 Policy: Encourage single-family development.

Consistency: All of the residential area is proposed as single family detached homes.

7.2.8 Policy: Develop one-time fee structure

Consistency: Development of fee structure is beyond the scope of this Specific Plan.

7.2.9 Policy: Require fiscal analysis in conjunction with EIRs.

Consistency: A fiscal analysis of the proposed project will be prepared in the EIR for the project.

7.2.10 Policy: Maintain target industry list

Consistency: NA: A list of target industries for the City of Fontana is beyond the scope of this Specific Plan.

7.2.11 Policy: Maintain liaison with Chamber of Commerce.

Consistency: Project applicant maintains liaison with the Chamber of Commerce through membership in the chamber.

7.2.12 Policy: Develop fiscal benefit model

Consistency: A fiscal analysis will be prepared as part of the project EIR.

7.2.13 Policy: State campus.

Consistency: NA: Development of a state campus in Fontana is beyond the scope of this Specific Plan proposal.

INFRASTRUCTURE ELEMENT

8.1.1 Policy: Development shall be phased in accordance with necessary infrastructure

Consistency: Development will be phased as described in the Specific Plan, in accordance with provision of necessary circulation improvements, utilities and services.

8.1.2 Policy: "Leapfrog" development not permitted.

Consistency: Proposed Specific Plan is in accordance with General Plan land use policy map, and development is contiguous with related land uses and the Rancho Fontana Specific Plan, already partially developed.

8.1.3 Policy: Financing plans for infrastructure improvements required.

Consistency: Financing plans for infrastructure will be proposed as part of the Master Capital Facilities Study and Plan or through a development agreement.

8.1.4 Policy: Master Capital Facilities Study and Plan.

Consistency: The City proposes to establish this plan prior to recordation of a subdivision map, and if the study is uncompleted through a GPA and development agreement.

8.2.1 Policy: Creation of Communities Facilities District (CFD) for North Fontana.

Consistency: If a CFD is formed, the project applicant, Kaufman and Broad may participate.

Water Policies:

8.3.1 Policy: Conservation and development of water resources Consistency:

Water conservation measures will be implemented within the Specific Plan per the mitigation measures in the EIR.

8.3.2 Policy: Prevent contamination of water resources.

Consistency: Although no known contamination of water resources exists on site, all necessary measures will be undertaken to guard against future contamination due to development.

8.3.3 Policy: "Grey Water" usage.

Consistency: Feasibility of reclaimed (grey) water usage will be investigated for park irrigation.

Sewers:

8.4.1 Policy: Develop standards for minimizing water use.

Consistency: Water conservation measures will be implemented. (See Section 5.2).

8.5.1 Policy: Investigate reuse options for RP-3 tertiary wastewater

Consistency: Reuse options for Regional Plan #3 tertiary wastewater is Beyond the scope of this Specific Plan. (See Policy 8.3.3).

Solid Waste:

8.6.1 Policy: Encourage recycling of solid wastes.

Consistency: Solid waste recycling could occur in conjunction with a supermarket program with the commercial center at California Landings or could occur community wide if the City provides a City-wide pick-up program.

Flood Control:

8.7.1 Policy: Support implementation of SBFCD Comprehensive Storm Drain Plan.

Consistency: Specific Plan drainage improvement will be coordinated in conjunction with SBFCD Comprehensive Storm Drain Plan.

8.7.2 Policy: Create communities facility district to equitably finance flood control infrastructure

Consistency: The applicant may participate in a CFD to finance flood control infrastructure if a CFD is created by the City.

8.7.3 Policy: Retention/detention ponds

Consistency: Due to the lack of regional detention facilities, temporary detention areas are proposed as flood control solutions, as indicated on the Storm Drainage Exhibit. (See Section 5.4) and will require a general plan amendment.

8.8.1 Policy: Encourage multi-use of flood control facilities for open space and recreation purposes

Consistency: A temporary detention area may be constructed if approved by the City Engineer.

PUBLIC SERVICES/FACILITIES ELEMENT

9.1.1 Policy: Maintain police/residents ratio

Consistency: Rates of 1.4 sworn police officers per 1,000 residents is being met by City of Fontana. This Specific Plan proposal would require 4 new officers for a population of 2,610.

9.1.2 Policy: Provide new police facilities

Consistency: California Landings will coordinate with the findings of the Capital Facilities Plan.

9.1.3 Policy: Fire Rating of Class 3

Consistency: The California Landings project will meet fire standards based on adopted criteria generated by the North Fontana Infrastructure Study and City Ordinances.

9.1.4 Policy: New fire station at Sierra and Slover.

Consistency: See 9.1.3.

9.1.5 Policy: Maintain fire protection level.

Consistency: See 9.1.3.

9.1.6 Policy: Fire and police response.

Consistency: Project proponent will work with city fire and police departments to achieve a 5 minute response times. A site within the commercial area will be discussed with the City for dedication in exchange for extraordinary amenity credit.

9.1.7 Policy: Attain Public Service standards.

Consistency: Project proponent will work with city school districts, libraries and health services to attain adequate standards to serve Specific Plan site.

9.1.8 Policy: Community hospital feasibility

Consistency: NA: Hospital feasibility is beyond the scope of the Specific Plan.

9.1.9 Policy: City Fire Department feasibility

Consistency: NA: feasibility of paramedic service is beyond scope of this Specific Plan.

9.1.10 Policy: Southridge library feasibility

Consistency: NA: feasibility of a Southridge library is beyond the scope of this Specific Plan.

9.2.1 Policy: Phase development in accordance with provision of adequate services and facilities

Consistency: The project will provide for implementation of infrastructure improvements, including water, sewer and storm drainage, in conjunction with each phase of development. (See Section 6.2)

9.2.2 Policy: Financing plans are required for capital improvements to public service facilities

Consistency: Costs and related financing will be provided through the establishment of an Assessment District, private funding, fees or other such funding vehicle available and/or pursuant to an adopted future master plan of infrastructure for North Fontana, and/or development agreement.

CONSERVATION ELEMENT

Water Resources:

10.1.1 Policy: Maintain and improve the quality and quantity of local and regional groundwater resources.

Consistency: California Landings will conform to standards and cooperate with city to achieve goal of maintaining quality and quantity of groundwater resources.

10.1.2 Policy: Water conservation measures must be incorporated into new developments.

Consistency: The proposed Specific Plan will comply with water conservation measures as required by law.

10.1.3 Policy: Require use of drought tolerant landscapes in new development; encourage replacement of water consumptive landscapes

Consistency: California Landings will utilize drought tolerant landscapes as described in the Specific Plan. (See Section 4.4)

10.1.4 Policy: Study reuse options of RP-3 treated water.

Consistency: See policy 8.5.1.

10.1.5 Policy: Encourage expeditious cleanup of contaminated soil and ground-water at Kaiser Steel site.

Consistency: NA: clean-up of Kaiser Steel site is beyond scope of this Specific Plan.

10.1.6 Policy: Encourage preservation of agricultural uses.

Consistency: Previously, 64 acres of the site had been used for poultry ranch, which has been abandoned in mid1991, and is no longer economically feasible. All the chicken ranch buildings were removed in mid1991 by the chicken ranch operator.

10.1.7 Policy: Reduce soil erosion. Requires erosion control plan for new development.

Consistency: Prior to grading and construction, an erosion control plan will be submitted and approved as per City policies. Project will come into compliance with the Clean Water Act.

10.1.8 Policy: Encourage development of community gardens in multi-family and planned unit developments.

Consistency: Community garden areas may be designated into the required open space in the single-family attached residential areas of the project, if required.

10.1.9 Policy: Minimize grading impacts.

Consistency: The site is flat, therefore grading impacts will be minimal.

10.1.10 Policy: Require feasibility study for curbside recycling program.

Consistency: NA: Beyond the scope of this Specific Plan.

10.1.11 Policy: Support expansion of Fontana Landfill.

Consistency: NA: Beyond the scope of this Specific Plan.

10.2.1 Policy: Minimize biotic impact.

Consistency: The California Landings EIR will address biotic impacts and mitigation, if appropriate.

10.2.2 Policy: Establish wildlife corridors in City

Consistency: California Landings Specific Plan is not located in an area appropriate for establishment of wildlife corridors.

10.2.3 Policy: Encourage tree planting.

Consistency: Enhanced landscape parkways will be provided along circulation corridors as described in the Specific Plan, to enhance the environment. (See Section 4.4).

10.3.1 Policy: Encourage use of alternative energy resources, e.g. solar energy.

Consistency: The project will encourage the use of alternate energy sources, as described in the Specific Plan.

10.3.2 Policy: Encourage design techniques which conserve energy.

Consistency: California Landings will encourage an energy-efficient community through energy conservation measures and design concepts.

10.4.1 Policy: Preservation of archaeological resources.

Consistency: An archaeological investigation will be performed for the project EIR, and city policies will be followed if resources are found.

10.4.2 Policy: Adoptive reuse of historical buildings.

Consistency: NA: no known historical structures exist on the site.

10.5.1 Policy: Aggregate resources.

Consistency: NA: no aggregate resource areas are proposed for mining on this site.

10.5.2 Policy: Reclamation of mined lands.

Consistency: NA: there are no mined lands on-site, therefore reclamation does not apply.

10.5.3 Policy: Mineral production mitigation.

Consistency: NA: no mineral production is proposed.

10.5.4 Policy: Aggregate resource extraction hazards.

Consistency: Aggregate resource extraction is not proposed on-site, therefore eliminating residual hazards to public health and safety.

OPEN SPACE ELEMENT

11.1.1 Policy: Balance natural and recreational open space

Consistency: California Landings proposes improved recreational open space (neighborhood park, parkways and bike trails) to contribute to the city's inventory of open space. (See Section 3-5)

11.1.2 Policy: Preserve significant natural habitat areas

Consistency: NA: no significant environmental area of the Jurupa Hills or San Gabriel Mountains exist on -site.

11.2.1 Policy: Require open space and recreation areas with new residential development

Consistency: Recreation area (park) is proposed within the new residential development within the Specific Plan (see 11.2.2 below).

11.2.2 Policy: Establish new neighborhood parks

Consistency: One new park is proposed within the Specific Plan. The park is a 10.0 acre park, centrally located within walking distance of residential area on site. (See Section 3-5.)

11.2.3 Policy: Develop parks in conjunction with schools

Consistency: An elementary school site is proposed within the project site, and a large central park is proposed adjacent to Beech Avenue.

1121 Policy: Ensure park design meets user needs.

Consistency: The park provided will be designed to meet needs of surrounding residents by providing adequate facilities and access as defined in General Plan park standards.

1122 Policy: Define additional cultural facilities.

Consistency: No additional cultural facilities are proposed within the Specific Plan area at this time.

1123 Policy: Park standards.

Consistency: According to city standards, 5.2 acres of community park (2 acres /1,000 people x 2,610 persons) are required and 7.8 acres of neighborhood park (3 ac/1,000) are required, for a total of 13.0 acres. A total of 10.0 acres of park land are provided for dedication.

1124 Policy: Prepare Parks Master Plan.

Consistency: NA: City to prepare master plan, not within the scope of this Specific Plan.

11.3.1 Policy: Preserve hilly areas.

Consistency: NA: the site is flat, with no hilly areas or areas exceeding 30% slope to preserve.

11.4.1 Policy: Establish open space/recreation networks.

Consistency: As described in the Specific Plan, circulation corridors proposed will provide for pedestrian and bicycle path as linkages through the site and to off-site areas. (See Section 3-5).

SAFETY ELEMENT

12.1.1 Policy: Restrict development from slope and flood areas.

Consistency: NA: site is not within areas of 30% or greater slope. Site is not in an area subject to flooding.

12.1.2 Policy: Restrict development in Alquist-Priolo zone areas.

Consistency: NA: proposed development is not within an Alquist-Priolo Zone.

12.1.3 Policy: Fire management plans in areas subject to wildfires.

Consistency: NA: proposed development is not within an area subject to wildfires.

12.2.1 Policy: Survey all unreinforced masonry structures.

Consistency: NA: structures on site are to be removed.

12.2.2 Policy: Continue updating of emergency development plan.

Consistency: If emergency evacuation is determined to be an issue for the proposed development, the Specific Plan will outline a project specific emergency preparedness plan before final approvals.

12.3.1 Policy: Support Federal, State, and County agencies.

Consistency: The Specific Plan will conform to current laws of federal, state and county agencies responsible for health and safety.

12.32 Policy: Fire and Police development review re: emergency services.

Consistency: The Specific Plan will be reviewed by county fire and city police departments.

12.33 Policy: Ensure safe construction

Consistency: City construction standards will be met in all proposed development at California Landings.

12.41 Policy: Enforce disclosure laws re: hazardous materials

Consistency: NA: No hazardous material will be permitted. No disclosure required.

12.42 Policy: Identify hazardous material transport routes.

Consistency: NA: Not within the scope of this Specific Plan.

12.43 Policy: Cooperate with railroads re: hazardous Emergency Plan materials.

Consistency: NA: Not within the scope of this Specific Plan.

12.44 Policy: Maintain separation between hazardous materials and areas where people live or congregate.

Consistency: No hazardous materials handling or transport are proposed and none exist in this surrounding area, therefore California Landings will conform with this policy.

12.45 Policy: Commercial and industrial uses must participate in hazardous materials mitigation.

Consistency: No industrial facilities are proposed, and commercial facilities in the Specific Plan will not be permitted to contain hazardous materials.

12.46 Policy: Encourage cleanup at the Kaiser Steel site.

Consistency: NA: clean-up of Kaiser site is beyond the scope of this Specific Plan

NOISE ELEMENT

13.1.1 Policy: Construct barriers, participate in abatement plans

Consistency: The Specific Plan will evaluate noise impacts in its EIR, which will also propose mitigation measures such as barriers where necessary.

13.1.2 Policy: Include noise mitigation in new roadway projects

Consistency: Noise mitigation measures will be included in design with new roadways proposed in the Specific Plan, as per state and local noise standards.

13.1.3 Policy: Reduce transportation noise

Consistency: New roadways proposed in the Specific Plan will be designed and coordinated to reduce noise.

13.1.4 Policy: Enforce noise laws

Consistency: NA: enforcement of noise levels by city divisions is the city's responsibility.

13.2.1 Policy: Establish community noise ordinance

Consistency: NA: not within the scope of this Specific Plan.

13.2.2 Policy: Monitor Rialto Airport noise

Consistency: Aircraft operations at Rialto Airport do not affect the Specific Plan site.

13.2.3 Policy: Encourage acoustical design to meet noise standards

Consistency: The Specific Plan will encourage acoustical design in new construction to meet noise attenuation standards.

13.3.1 Policy: Establish a new Community Noise Ordinance

Consistency: NA: establishment of a new community Noise Ordinance is outside the scope of this Specific Plan.

AIR QUALITY ELEMENT

14.11 Policy: Cooperate with SCAQMD

Consistency: California Landings Specific Plan will cooperate with SCAQMD to implement the standards of the regional air quality management plan for the proposed project.

14.12 Policy: Enforce City Ordinances re: industrial emissions

Consistency: NA: no industrial land uses are proposed.

14.13 Policy: Require Transportation Systems Management Plans for major Projects.

Consistency: NA: not within the scope of this Specific Plan.

14.1.4 Policy: Review major projects re: air quality impacts

Consistency: The Specific Plan will be reviewed by the city to determine potential air quality impacts.

14.1.5 Policy: Work with State Air Resources Board and SCAQMD re: Lewis Clean Air Act

Consistency: The Specific Plan will conform to ensure implementation of Lewis Clean Air Act.

7.3 PLANT PALETTE

CALIFORNIA LANDINGS MASTER PLANT LIST

Note:

- Plant material selections are preliminary. Final type and location to be determined in final design.
- Plant material subject to change based on final agronomy test and soils report.

PRIMARY ENTRY

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	<u>(D)*</u> <u>(E)</u>	<u>D.T.**</u> <u>(Y/N)</u>
<u>TREES</u>					
<i>Calodendron capense</i>	Cape Chestnut	single specimen	varies	E	N
<i>Cinnamomum camphora</i>	Camphor Tree	single specimen	varies	E	N
<i>Lagerstroemia indica</i>	Crape Myrtle	20'	36" box	D	Y
<i>Liquidambar styraciflua</i>	Sweet Gum	20'	24" box	D	N
<i>Magnolia grandiflora</i>	Southern Magnolia	single specimen	varies	E	N
<i>Pinus spp.</i>	Pine	20'	24" box	E	Y
<i>Prunus cerasifera</i> 'Atropurpurea'	Purple Leaf Plum	15'	36" box	D	N
<i>Pyrus calleryana</i>	Flowering Pear	20'	36" box	D	N

* (D) Deciduous, (E) Evergreen.

** D.T. Drought Tolerant - (Y) Yes, (N) No.

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	(D)*	D.T.**
<u>TREES</u> cont.					
<i>Quercus spp.</i>	Oak	single specimen	varies	E	Y

<i>Ulmus parvifolia</i>	Evergreen Elm	single specimen	varies	D	Y
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SECONDARY ENTRY

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	(D)*	D.T.**
<u>TREES</u>					
<i>Calodendron capense</i>	Cape Chestnut	single specimen	varies	E	N
<i>Cinnamomum camphora</i>	Camphor Tree	single specimen	varies	E	N
<i>Lagerstroemia indica</i>	Crape Myrtle	20'	36" box	D	Y
<i>Liquidambar styraciflua</i>	Sweet Gum	20'	24" box	D	N
<i>Magnolia grandiflora</i>	Southern Magnolia	single specimen	varies	E	N
<i>Pinus spp.</i>	Pine	20'	24" box	E	Y
<i>Prunus cerasifera</i> 'Atropurpurea'	Purple Leaf Plum	15'	36" box	D	N
<i>Pyrus calleryana</i>	Flowering Pear	20'	36" box	D	N

* (D) Deciduous, (E) Evergreen.

** D.T. Drought Tolerant - (Y) Yes, (N) No.

NEIGHBORHOOD ENTRY (by merchant builder)

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	(D)* (E)	D.T.** (Y/N)
TREES					
<i>Lagerstroemia indica</i>	Crape Myrtle	20'	15 gal.	D	Y
<i>Liquidambar styraciflua</i>	Sweet Gum	20'	15 gal.	D	N
<i>Magnolia grandiflora</i>	Southern Magnolia	20'	15 gal.	E	N
<i>Pinus spp.</i>	Pine	20'	15 gal.	E	Y
<i>Prunus cerasifera</i> 'Atropurpurea'	Purple Leaf Plum	15'	15 gal.	D	N
<i>Pyrus calleryana</i>	Flowering Pear	20'	15 gal.	D	N

HIGHLAND AVENUE

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	(D)* (E)	D.T.** (Y/N)
TREES					
Parkway:					
<i>Alnus rhombifolia</i>	White Alder	15'	15 gal.	D	N
<i>Brachychiton populneus</i>	Bottle Tree	25'	15 gal.	E	Y
<i>Cinnamomum camphora</i>	Camphor Tree	50'	15 gal.	E	N
<i>Cupressus sempervirens</i>	Italian Cypress	10'	15 gal.	E	Y
<i>Eucalyptus spp.</i>	Eucalyptus	15'	15 gal.	E	Y

* (D) Deciduous, (E) Evergreen.

** D.T. Drought Tolerant - (Y) Yes, (N) No.

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	<u>(D)*</u>	<u>D.T.**</u>
<u>TREES cont.</u>					
<i>Geijera parvifolia</i>	Australian Willow	25'	15 gal.	E	N
<i>Koelreuteria paniculata</i>	Goldrain Tree	40'	15 gal.	D	Y
<i>Lagerstroemia indica</i>	Crape Myrtle	20'	15 gal.	D	Y
<i>Liquidambar styraciflua</i>	Sweet Gum	20'	15 gal.	D	N
<i>Liriodendron tulipifera</i>	Tulip Tree	30'	15 gal.	D	N
<i>Pinus spp.</i>	Pine	20'	15 gal.	E	Y
<i>Platanus spp.</i>	Sycamore	40'	15 gal.	D	Y
<i>Prunus cerasifera 'Atropurpurea'</i>	Purple Leaf Plum	15'	15 gal.	D	N
<i>Pyrus spp.</i>	Flowering Pear	20'	15 gal.	D	N
<i>Quercus spp.</i>	Oak	40'	15 gal.	E	Y
<i>Shinus terebinthifolius</i>	Brazilian Pepper	30'	15 gal.	E	Y
<i>Tristaniopsis conferta</i>	Brisbane Box	25'	15 gal.	E	Y
<i>Ulmus parvifolia</i>	Evergreen Elm	30'	15 gal.	D	Y

* (D) Deciduous, (E) Evergreen.

** D.T. Drought Tolerant - (Y) Yes, (N) No.

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	(D)*	D.T.**
				(E)	(MN)

TREES cont.

Median:

<i>Calodendron capense</i>	Cape Chestnut	40'	15 gal.	E	N
<i>Cinnamomum camphora</i>	Camphor Tree	40'	15 gal.	E	N
<i>Magnolia grandiflora</i>	Southern Magnolia	20'	15 gal.	E	N
<i>Pinus pinea</i>	Italian Stone Pine	40'	15 gal.	E	Y
<i>Quercus spp.</i>	Oak	40'	15 gal.	E	Y

BEECH AVENUE

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	(D)*	D.T.**
				(E)	(MN)

TREES

Parkway:

<i>Alnus rhombifolia</i>	White Alder	15'	15 gal.	D	N
<i>Brachychiton populneus</i>	Bottle Tree	25'	15 gal.	E	Y
<i>Cinnamomum camphora</i>	Camphor Tree	50'	15 gal.	E	N
<i>Geijera parvifolia</i>	Australian Willow	25'	15 gal.	E	N
<i>Koelreuteria paniculata</i>	Goldenrain Tree	40'	15 gal.	D	Y
<i>Liquidambar styraciflua</i>	Sweet Gum	20'	15 gal.	D	N

* (D) Deciduous, (E) Evergreen.

** D.T. Drought Tolerant - (Y) Yes, (N) No.

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	<u>(D)*</u>	<u>D.T.**</u>
<u>TREES cont.</u>					
<i>Pinus spp.</i>	Pine	20'	15 gal.	E	Y
<i>Platanus spp.</i>	Sycamore	40'	15 gal.	D	Y
<i>Pyrus spp.</i>	Flowering Pear	20'	15 gal.	D	N
<i>Quercus spp.</i>	Oak	40'	15 gal.	E	Y
<i>Shinus terebinthifolius</i>	Brazilian Pepper	30'	15 gal.	E	Y
<i>Tristania conferta</i>	Brisbane Box	25'	15 gal.	E	Y
<i>Ulmus parvifolia</i>	Evergreen Elm	30'	15 gal.	D	Y
<u>Median:</u>					
<i>Calodendron capense</i>	Cape Chestnut	40'	15 gal.	E	N
<i>Cinnamomum camphora</i>	Camphor Tree	40'	15 gal.	E	N
<i>Magnolia grandiflora</i>	Southern Magnolia	20'	15 gal.	E	N
<i>Pinus pinea</i>	Italian Stone Pine	40'	15 gal.	E	Y
<i>Quercus spp.</i>	Oak	40'	15 gal.	E	Y
					Y

* (D) Deciduous, (E) Evergreen.

** D.T. Drought Tolerant - (Y) Yes, (N) No.

WALNUT AVENUE (east and west of Beach)

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	(D)* (E)	D.T.** (Y/N)
<u>TREES</u>					
<i>Bauhinia variegata</i>	Purple Orchid Tree	25'	15 gal.	D	N
<i>Calodendron capense</i>	Cape Chestnut	40'	15 gal.	E	N
<i>Cercis occidentalis</i>	Western Redbud	15'	15 gal.	D	Y
<i>Cinnamomum camphora</i>	Camphor Tree	40'	15 gal.	E	N
<i>Lagerstroemia indica</i>	Grape Myrtle	20'	15 gal.	D	Y
<i>Magnolia grandiflora</i>	Southern Magnolia	20'	15 gal.	E	N
<i>Pinus spp.</i>	Pine	20'	15 gal.	E	Y
<i>Prunus cerasifera</i> 'Atropurpurea'	Purple Leaf Plum	15'	15 gal.	D	N
<i>Quercus spp.</i>	Oak	40'	15 gal.	E	Y

NEIGHBORHOOD COLLECTOR (Non-Loaded)

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	(D)* (E)	D.T.** (Y/N)
<u>TREES</u>					
<i>Alnus rhombifolia</i>	White Alder	75'	15 gal.	D	N
<i>Cupressus sempervirens</i>	Italian Cypress	20'	15 gal.	E	Y
<i>Eucalyptus spp.</i>	Eucalyptus	15'	15 gal.	E	Y

* (D) Deciduous, (E) Evergreen.

** D.T. Drought Tolerant - (Y) Yes, (N) No.

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	(D)* (E)	D.T.** (Y/N)
<u>TREES cont.</u>					
<i>Liriodendron tulipifera</i>	Tulip Tree	40'	15 gal.	D	N
<i>Magnolia grandiflora</i>	Southern Magnolia	35'	15 gal.	E	N
<i>Pinus eldarica</i>	Mondell Pine	25'	15 gal.	E	Y
<i>Populus nigra 'Italica'</i>	Lombardy Poplar	30'	15 gal.	D	N
<i>Schinus terebinthifolius</i>	Brazilian Pepper	30'	15 gal.	E	Y

HEMLOCK AVENUE

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	(D)* (E)	D.T.** (Y/N)
<u>TREES</u>					
<i>Cedrus deodora</i>	Deodar Cedar	40'	15 gal.	E	N
<i>Eucalyptus spp.</i>	Eucalyptus	varies	15 gal.	E	Y
<i>Liquidambar styraciflua</i>	Sweet Gum	20'	15 gal.	D	N
<i>Liriodendron tulipifera</i>	Tulip Tree	30'	15 gal.	D	N
<i>Platanus spp.</i>	Sycamore	40'	15 gal.	D	Y
<i>Pinus spp.</i>	Pine	20'	15 gal.	D	Y
<i>Populus nigra 'Italica'</i>	Lombardy Poplar	15'	15 gal.	D	N

* (D) Deciduous, (E) Evergreen.
 ** D.T. Drought Tolerant - (Y) Yes, (N) No.

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	<u>(D)*</u> <u>(E)</u>	<u>D.T.**</u> <u>Y/N</u>
<u>SHRUBS</u>					
<i>Abeila grandiflora</i>	Glossy Abelia	4-6'	1 or 5 gal.	E	Y
<i>Acacia redolens</i>	N.C.N.	8'	1 gal.	E	Y
<i>Arbutus unedo</i>	Strawberry Tree	8-10'	5 gal.	E	N
<i>Asparagus meyeri</i>	Meyer's Asparagus	2-3'	1 gal.	E	Y
<i>Baccharis pilularis</i>	Coyote Bush	4-6'	1 gal.	E	Y
<i>Buxus microphylla</i> 'Japonica'	Japanese Boxwood	3-4'	1 or 5 gal.	E	N
<i>Cercis occidentalis</i>	Western Redbud	6-8'	1 or 5 gal.	D	Y
<i>Ceanothus spp.</i>	Varies	varies	1 or 5 gal.	E	Y
<i>Cistus purpureus</i>	Rockrose	4'	1 or 5 gal.	E	Y
<i>Coleonema pulchrum</i>	Pink Breath of Heaven	5'	1 or 5 gal.	E	N
<i>Cotoneaster parneyi</i>	Cotoneaster	8'	1 or 5 gal.	E	Y
<i>Dietera bicolor</i>	Fortnight Lily	5'	1 or 5 gal.	E	N
<i>Euonymous japonica</i>	Evergreen Euonymous	5'	1 or 5 gal.	E	Y
<i>Feijoa sellowiana</i>	Pineapple Guava	15'	5 gal.	E	Y
<i>Grevillea noelii</i>	Noel's Grevillea	4'	1 or 5 gal.	E	Y
<i>Heteromeles arbutifolia</i>	Toyon	8'	1 or 5 gal.	E	Y
<i>Ilex cornuta</i>	Chinese Holly	6-8-10'	1 or 5 gal.	E	Y

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<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SPACING</u>	<u>SIZE</u>	<u>(D)*</u> <u>(E)</u>	<u>D.T.**</u> <u>(Y/N)</u>
<u>SHRUBS cont.</u>					
<i>Leptospermum scoparium</i>	New Zealand Tea Tree	6-8'	1 or 5 gal.	E	Y
<i>Ligustrum japonicum 'Texanum'</i>	Japanese Privet	3-5'	1 or 5 gal.	E	N
<i>Nerium oleander</i>	Oleander	6-8'	1 or 5 gal.	E	Y
<i>Pennisetum spp.</i>	Fountain Grass	varies	1 or 5 gal.	E	Y
<i>Photinia fraseri</i>	Photinia	8"	1 or 5 gal.	E	N
<i>Phormium tenax</i>	New Zealand Flax	6'	1 or 5 gal.	E	Y
<i>Pittosporum tobira</i>	Mock Orange	8"	1 or 5 gal.	E	Y
<i>Punica granatum 'Nana'</i>	Dwarf Pomegranate	3'	1 or 5 gal.	D	N
<i>Pyracantha spp.</i>	Pyracantha	varies	1 or 5 gal.	E	Y
<i>Prunus caroliniana</i>	California Laurel Cherry	6'	1 or 5 gal.	E	Y
<i>Raphiolepis indica</i>	India Hawthorn	4'	1 or 5 gal.	E	N
<i>Rosmarinus officinalis 'Prostratus'</i>	Rosemary	3-5'	1 gal.	E	Y
<i>Santolina spp.</i>	Lavender Cotton	varies	1 or 5 gal.	E	Y
<i>Soliva heterophylla</i>	Australian Bluebell Creeper	3-4'	1 or 5 gal.	E	N
<i>Spirea bumalda</i>	Spirea	35"	1 or 5 gal.	E	N

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				(E)	M/N
<u>SHRUBS cont.</u>					
<i>Tecomania capensis</i>	Cape Honeysuckle	6-8'	1 or 5 gal.	E	Y
<i>Trachelospermum jasminoides</i>	Star Jasmine	3'	1 gal.	E	N
<i>Xylosma congestum</i>	Shiny Xylosma	8'	1 or 5 gal.	E	Y
<u>GROUNDCOVERS</u>					
<i>Arctostaphylos</i> spp.	Varies	varies	flats/ containers	E	Y
<i>Carissa grandiflora</i>	Natal Plum	18-24"	1 gal.	E	N
<i>Hedera helix</i> 'Henzii'	Hahn's Ivy	8-12"	flats	E	N
<i>Hypericum calycinum</i>	Aaron's Beard	8-12"	flats	E	N
<i>Lippia canescens</i>	Lippia	8-12"	flats	E	Y
<i>Lonicera japonica</i>	Japanese Honeysuckle	12-18"	flats	E	N
<i>Pyracantha</i> 'Santa Cruz'	Santa Cruz Pyracantha	3-4'	1 gal.	E	N
<i>Rosmarinus officinalis</i> 'Prostratus'	Rosemary	3-4'	1 gal.	E	Y
<i>Vinca major</i>		12-18"	flats	E	N
<u>VINES</u>					
<i>Anemone leptopus</i>	Rosa de Montana	varies	1 or 5 gal.	D	Y
<i>Distictus buccinatoria</i>	Blood Red Trumpet Vine	varies	1 or 5 gal.	E	N

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				(E)	(MN)
<u>VINES cont.</u>					
<i>Ficus pumila</i>	Creeping Fig	varies	1 gal.	E	N
<i>Gelsemium sempervirens</i>	Carolina Jessamine	varies	1 or 5 gal.	E	Y
<i>Hardenbergia violacea</i>	N.C.N.	varies	1 or 5 gal.	E	Y
<i>Parthenocissus tricuspidata</i>	Virginia Creeper	varies	1 gal.	D	N
<i>Rosa banksiae</i>	Lady Banks Rose	varies	1 or 5 gal.	E	Y

7.5 PURPOSE OF THE SPECIFIC PLAN

The purpose of this document is to ensure the responsible development of the California Landings property through regulation based on thorough and comprehensive land use planning. The Specific Plan, when adopted by City legislative action, serves both as a planning function and a regulatory function as well as establishing infrastructure service needs. It will be the major device for implementing the General Plan on this property.

It is also a purpose of this plan to ensure responsiveness to the unique conditions, opportunities and constraints which apply to this particular property at this point in the development of the north Fontana area.

7.6 CALIFORNIA ENVIRONMENTAL QUALITY ACT COMPLIANCE

This Specific Plan is accompanied by a complete Environmental Impact Report. It has been prepared in accordance with the most recently adopted guidelines of the State of California and City of Fontana. The EIR contains an introduction, description of existing conditions, assessment of environmental impacts, identification of mitigation measures, and evaluation of project alternatives, as well as supporting technical documentation.

The EIR covers all elements of the California Landings Specific Plan, as well as its Design Guidelines and implementation mechanisms. The EIR is also applicable to future development projects (development plans, tentative tract maps, conditional use permits, etc.) which are processed in conformance with this Specific Plan.

7.7 AUTHORITY AND SCOPE

The California Government Code authorizes cities to adopt specific plans either by resolution as policy or by ordinance as regulation. Both Planning Commission and City council hearings are required. It must be adopted by the City council to be in effect.

This is a regulatory specific plan. It serves as zoning for the property involved. Development plans and tentative tract maps or tentative parcel maps in this area must be consistent with this specific plan. If a project is consistent with this specific plan it is also consistent with the General Plan. If a development agreement is sought, it must also be found to be consistent with the general plan and this specific plan.

The scope of subjects covered is the same as the scope of the general plan, to the extent they apply to this planning area.

The California Landings Specific Plan is established through the authority granted to the City of Fontana by the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457. The preparation of a specific plan for the California Landings area is also mandated by the City.